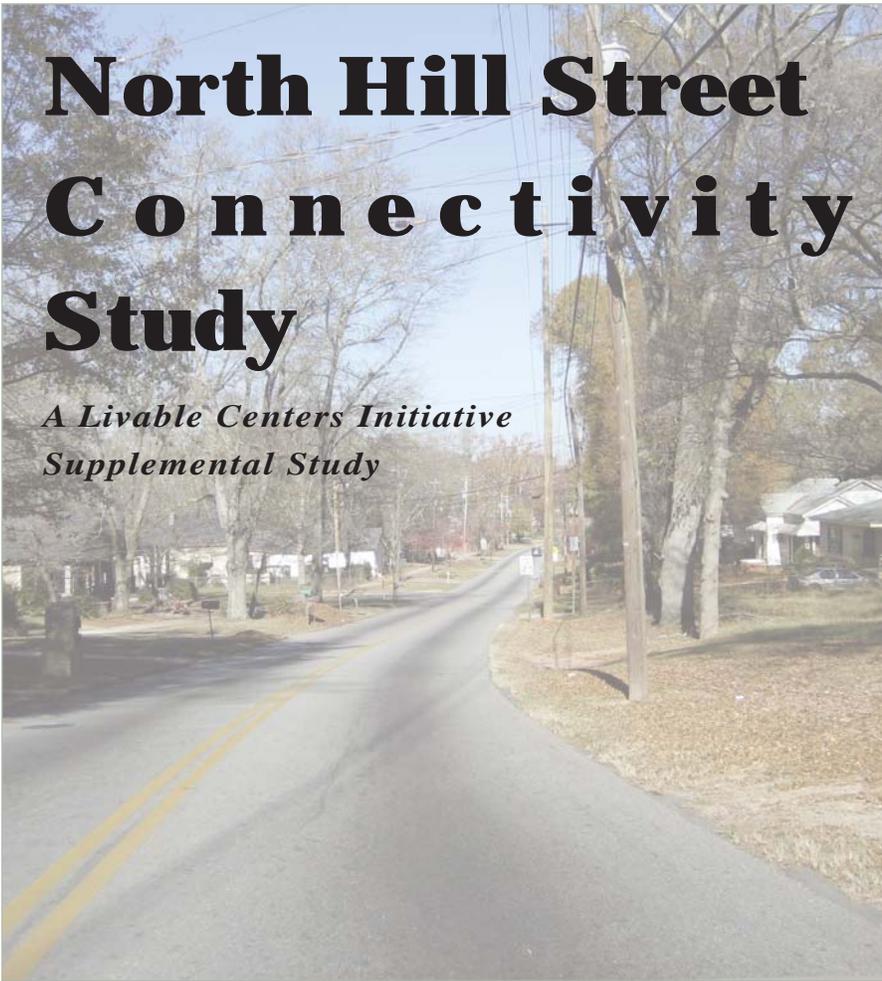


North Hill Street Connectivity Study

*A Livable Centers Initiative
Supplemental Study*



Sponsored by:
The City of Griffin, Spalding County &
the Minerva Group



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March 31, 2008

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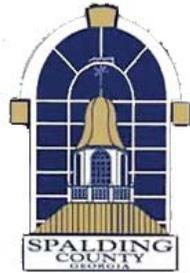
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I. Introduction

Purpose

The North Hill Street Connectivity Study is intended to build upon the recommendations of the 2006 City of Griffin Town Center Livable Centers Initiative (LCI) Study. The study, which included a review of transportation and land use in Griffin’s historic core and nearby neighborhoods, identified a need to improve pedestrian, vehicular, and bicycle connectivity between Downtown and the then-planned SunCity Peachtree development to the north. This connectivity would ensure that Downtown and SunCity Peachtree grow in a way that benefits both, as well as the areas in between them.

In late 2007 the Atlanta Regional Commission (ARC) awarded the City of Griffin funds to prepare a supplemental transportation and land use study of the area between Downtown and Jack Street. This study picks up where the LCI effort left off, and addresses transportation and land use in the LCI area at a level of detail not possible previously.

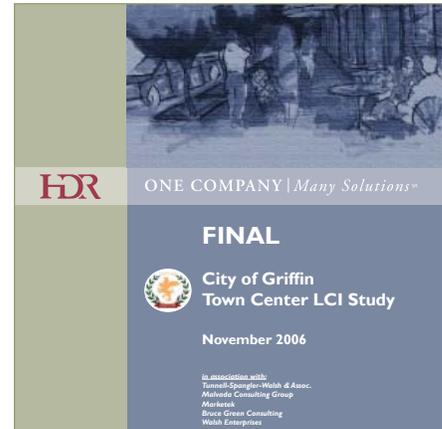
Concurrent with the supplemental study, the city and county, with support from the Minerva Group, embarked on a redevelopment plan for the greater North Hill Street corridor. The plan is intended to include the recommendations from this study, as well new ones, in a document meeting the requirements of O.C.G.A 36-44-2 et al. It provides the funding and statutory framework necessary to support the land use and transportation recommendations of both studies. The redevelopment plan also includes details on elements beyond the scope of transportation and land use.

Because of these concurrent efforts, the inventory, analysis, and some recommendations of this supplemental study are also included in the greater redevelopment plan. These are intended to reflect their integrated nature.

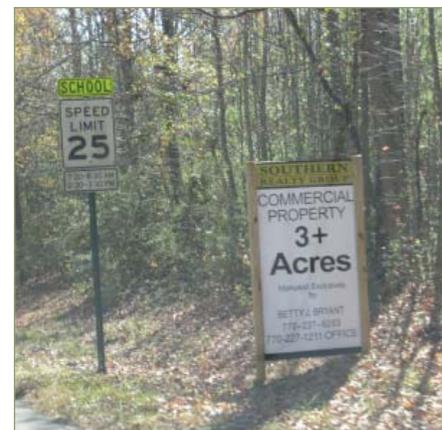
Location

The greater North Hill Street corridor lies in the City of Griffin and Spalding County, just north of Downtown Griffin. From its intersection with Broadway, the corridor extends 3.16 miles north to just past Dobbins Mill Road. Generally speaking, this 924 acre area includes the roadway itself and parcels along it. Of this Study Area, the supplemental study focusses south of Jack Street. Please see Figure 1 for specific boundaries.

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This study builds upon the recommendations of the 2006 Town Center LCI Study

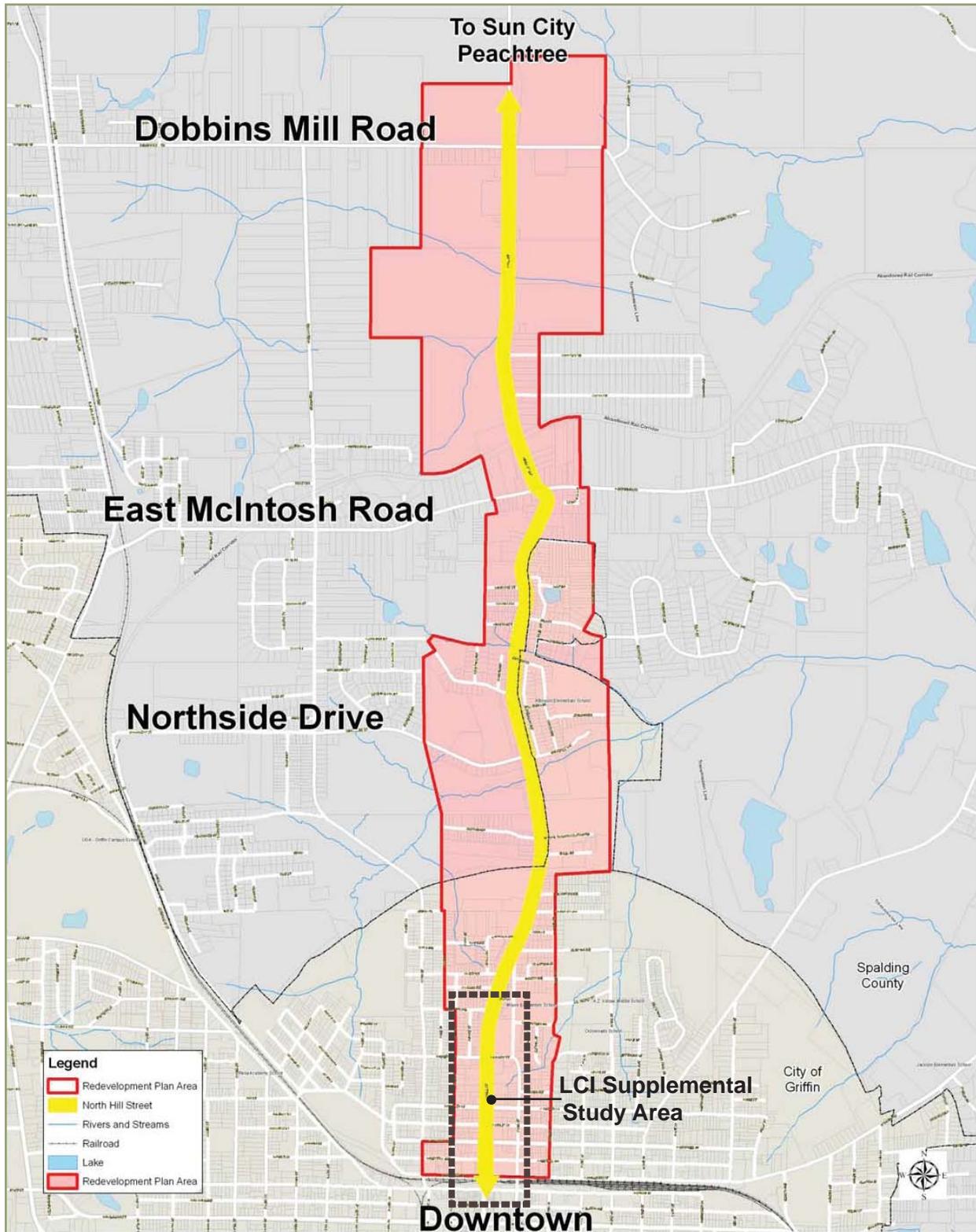


This supplemental study supports a long-term vision for transportation and land use along North Hill Street



The redevelopment plan will help the vision of this supplemental study become a reality

Figure 1: Study Area Overview



II. Land Use Conditions

Land uses and the relationship between them impact the quality of life in a community. Different land uses have varying impacts on transportation and utility systems. The arrangement of land uses and their proximity also support or discourage different modes of transportation, including bicycling and walking; this can directly impact the vehicular system by reducing or increasing traffic.

Cities such as Griffin were traditionally built as mixed-use environments featuring housing, shops, offices, religious institutions, schools, parks, and factories all within a short walk of one another. As the benefits of mixed-use areas are rediscovered, it is increasingly important to understand the uses that can operate within an acceptable walking distance. Many uses are compatible, including retail, office, open space, civic, and residential uses. Others, such as industrial and transportation services, are more difficult to reconcile in a mixed-use setting.

Existing Land Uses

The North Hill Street corridor contains a mix of land use ranging from a finely-mixed urban pattern at its southern end, to an almost rural pattern at its northern. Generally speaking, the former is found in the City of Griffin (in the supplemental study area), while the latter is in Spalding County. A detailed break-out can be found below.

Table 1: Existing Land Use Mix

Land Use	Parcels	Acres	Percent of Study Area
Agricultural	5	21.1	5.0%
Rural Reserve	3	11.8	2.8%
Open Space	44	21.1	5.0%
Low Density Residential	12	21.9	5.2%
Medium Density Residential	281	177.4	41.7%
Multi-family Residential	5	12.9	3.0%
Commercial	36	39.4	9.3%
Industrial	4	12.8	3.0%
Institutional/Public	5	16.8	3.9%
Transportation*	18	30.5	7.2%
Vacant/Undeveloped	48	59.8	14.1%
Total	461	425.4	100.0%

*Excludes street and rail rights-of-way



Historic buildings strengthen the downtown character at the southern end of the corridor

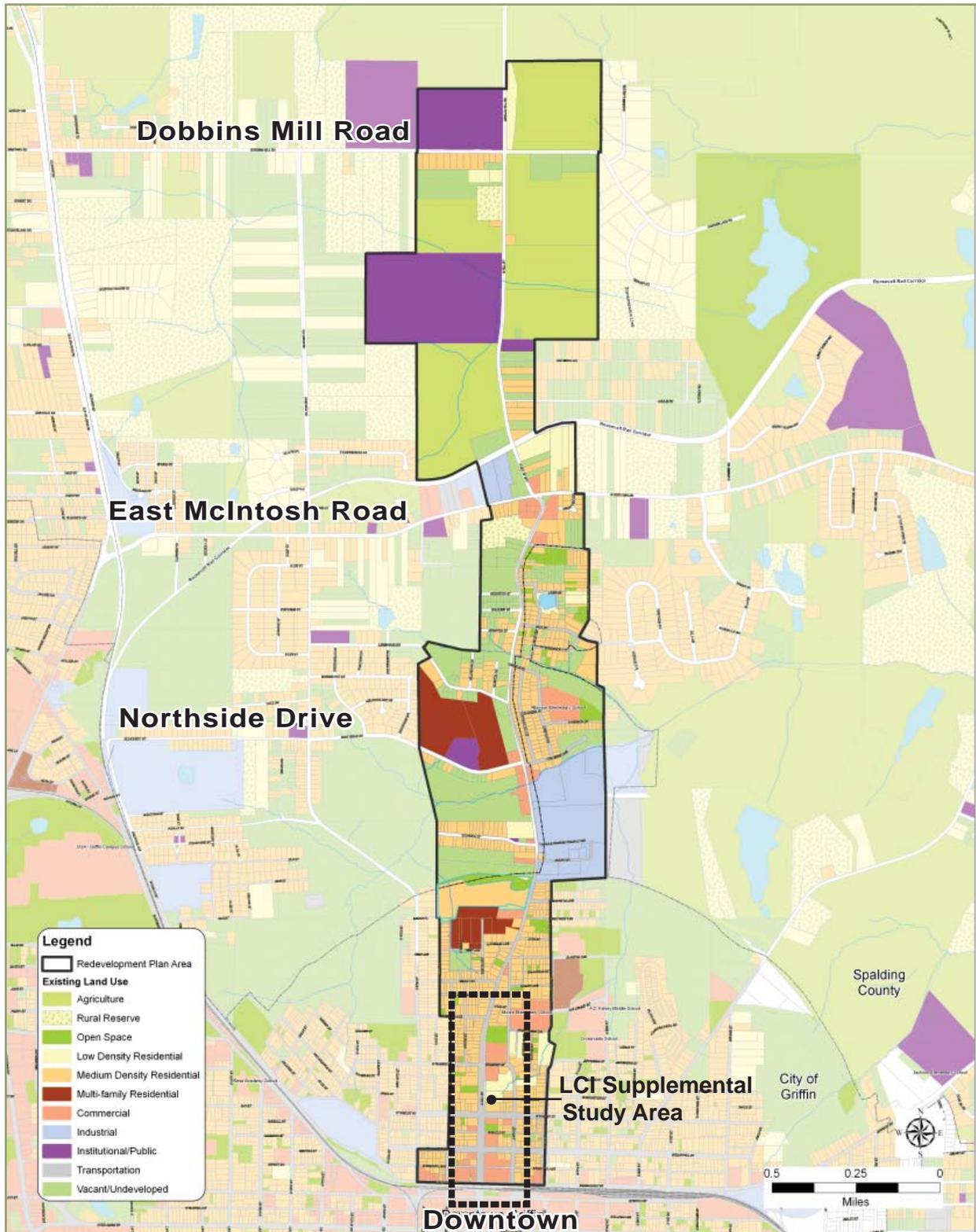


As Hill Street continues north, a variety of land uses are found



Medium Density Residential accounts for only 42% of land along the corridor

Figure 2: Existing Land Uses



At the corridor's southern end, commercial and medium density residential land uses are found, along with a few low density residential properties, schools, and parks. At the middle of the Study Area, multifamily residential are found on the west side of North Hill Street, with industrial land uses to the east. Many properties in this area are also vacant or undeveloped. The northern end of the corridor is marked by agricultural and institutional/public land uses. This northern area is also subject to development pressure; its large tracts of land could accommodate growth and support future activity near the SunCity Peachtree project.

Within this land use pattern there are several different activity nodes, including: Downtown, at Dobbins Mill Road, at East McIntosh Road, at Tuskegee Avenue, and at Blanton Avenue. These could be focal points of development or redevelopment.

Strengths

- The fine-grained mix of land uses near Downtown, which encourages walking and minimizes travel distance
- Historic neighborhoods, which provide a sense-of-place

Weaknesses

- Auto-oriented development patterns along the corridor's northern half, which limit walking and bicycling feasibility
- High percentage of rental housing, which destabilizes neighborhoods just north of Downtown
- Reverse frontage of buildings north of Tuskegee Avenue, which create a poor street character
- Lack of neighborhood-serving uses and jobs; which forces residents (many without cars) to travel long distances

Opportunities

- Potential redevelopment of under-utilized and agricultural lands, which could support new growth
- Older commercial sites, which could be redeveloped
- Vacant Moore Elementary School, which could be converted to new uses
- Five existing activity nodes, which could provide neighborhood-serving uses and support growth

Threats

- Development outside of the Study Area, which could bring transportation and land use challenges



Multi-family land uses about commercial land uses along the corridor



Many key intersections are underutilized and have potential for redevelopment



Reverse frontage homes turn their back on Hill Street and face Spelman Avenue, creating a blank wall along the corridor

Existing Future Land Use Policies

The comprehensive plans for the City of Griffin and Spalding County establish future land use classifications for all areas of the city and county. The classifications need not comply with current on-the-ground land uses, but rather reflect desired long-term land uses. Under Georgia law, the future land use plan serves as the legal basis for rezoning activity on the part of a local government. Therefore, it is important that such a plan accurately reflects the desired vision for an area. In this way, these classifications should serve as a guide for directing public infrastructure improvements that support the desired future land use.

For the most part, the land use classifications in the corridor reflect its diverse uses. The southern portion of the corridor is “Downtown Hub” at Broadway, with “Mixed-Use” between Broad and Georgia Avenue. Surrounding this and to the north on the west to Northside Drive is “Medium Density Residential.” On the east side, a small “Industrial” classification exists south of Tuskegee Avenue.

North of the Tuskegee Avenue/Northside Drive intersection the classifications are less intense. Neighborhoods east of North Hill Street are classified “Low Density Residential.” To the west, classifications are “Commercial, Medium Density Residential,” and “Industrial.” The corridor’s north end is classified “Low Density Residential, Institutional/Public, Agriculture/Forestry,” and “Rural Reserve.” An open space network is called for along area creeks.

While the city’s “Mixed-Use” classification supports the creation of a nodal land use pattern on the corridor, it is important to note that Spalding County’s currently does not. However, the county does have a “Village Node” classification that is being used in other parts of the County and may have application here.

Please see Figure 3 for more details.

Strengths

- Current policies, which protect neighborhoods from commercial encroachment
- “Mixed-Use” classifications near downtown, which support neighborhood-serving uses
- “Open Space Network” classifications in the county, which preserve green corridors along creeks
- “Institutional” and “Institutional/Public” classifications, which



“Downtown Hub” is found around the historic core of Griffin

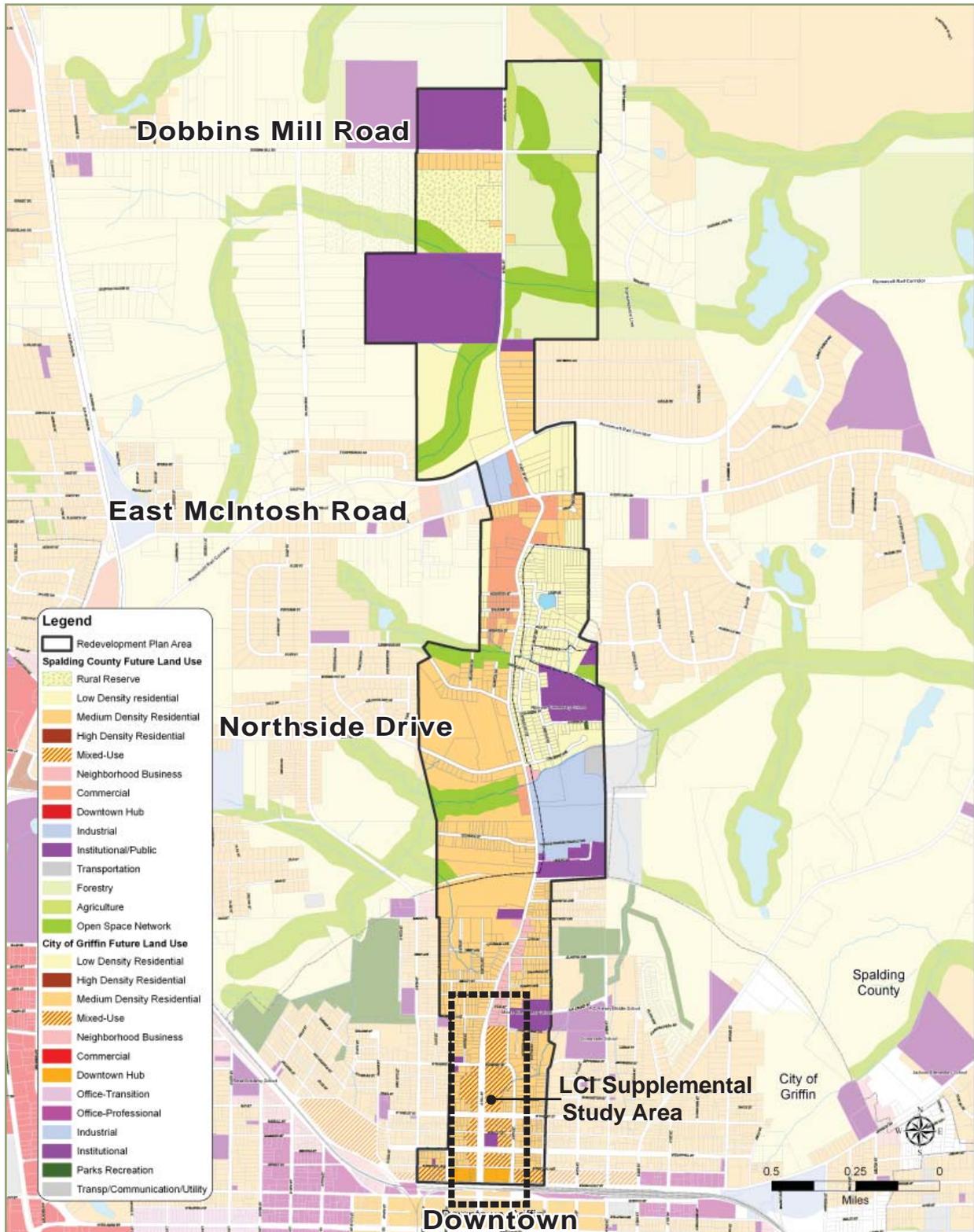


The “Medium Density Residential” classification can support condominiums and apartments



An “Industrial” classification is found on the east side of North Hill Street

Figure 3: Existing Future Land Use Plan



provide neighborhood civic uses

- “Industrial” classification south of Tuskegee Avenue, which provides jobs close to housing

Weaknesses

- Existing commercial areas on the west side of North Hill Street at Northside Drive are classified “Medium Density Residential,” which fails to recognize their role as important commercial uses for the surrounding residents
- A lack of any business-supporting (neighborhood or otherwise) classification at Dobbins Mill Road and North Hill Street, which fails to capitalize on access to Dundee Lake Park and the possibility for small park-related businesses

Opportunities

- Future land use plan changes, which could reflect the land use vision emerging from this process
- The county’s “Village Node” classification, which may have application at key intersections along the corridor

Threats

- Resistance to change

Existing Zoning

The City of Griffin and Spalding County regulate development through the use of zoning. Zoning districts control things such as height, use, setbacks, parking, etc. They are the implementation tool of the comprehensive plan and should support the desired land uses. Since zoning directly shapes development, it has a profound impact on the built environment. More than any other single element, zoning shapes how a community looks and functions for decades.

Because the corridor lies in two jurisdictions with different zoning districts, it contains a variety of zoning designations, as shown on Figure 4. For the most part, these reflect the land use classifications of the future land use plans. There is, however, a notable exception; the west side of North Hill Street between Northside Drive and East McIntosh Road (within Spalding County) is zoned C-2 Manufacturing. Yet much of this area is shown as “Medium Density Residential” on the future land use plan. This area also lies directly across North Hill Street from existing neighborhoods.

Within the City of Griffin, some zoning designations in the Study Area permit mixed use development by-right (CBD) or by special use permit (PRD and HDR-B). The county has no such designation in the corridor, although its VN Village Node district would permit such development. All other designations in both the city and county are single-use.

Current city and county zoning fails to provide thorough design standards to ensure that development is consistent with the historic character of Griffin or a more aesthetically pleasing and walkable land use pattern in the county. The city has general design standards, but they do not ensure compatible new development in historic areas.

Strengths

- CBD zoning allows for mixed-use, zero setback buildings in Downtown
- Existing residential districts, which prevent commercial encroachment into neighborhoods

Weaknesses

- Lack of design requirements, which has resulted in buildings that are out-of-character
- Current permission for frontal-parking for commercial uses, which creates an unpleasant streetscape
- C-2 Manufacturing zoning on North Hill Street, which is incompatible with its proximity to residential and its key connection between Downtown and SunCity Peachtree

Opportunities

- Zoning map changes, which could support appropriate new development densities
- The county's VN Village Node designation, which could be applied to areas where nodal commercial uses are desired

Threats

- No change to zoning, which would allow current zoning challenges to continue

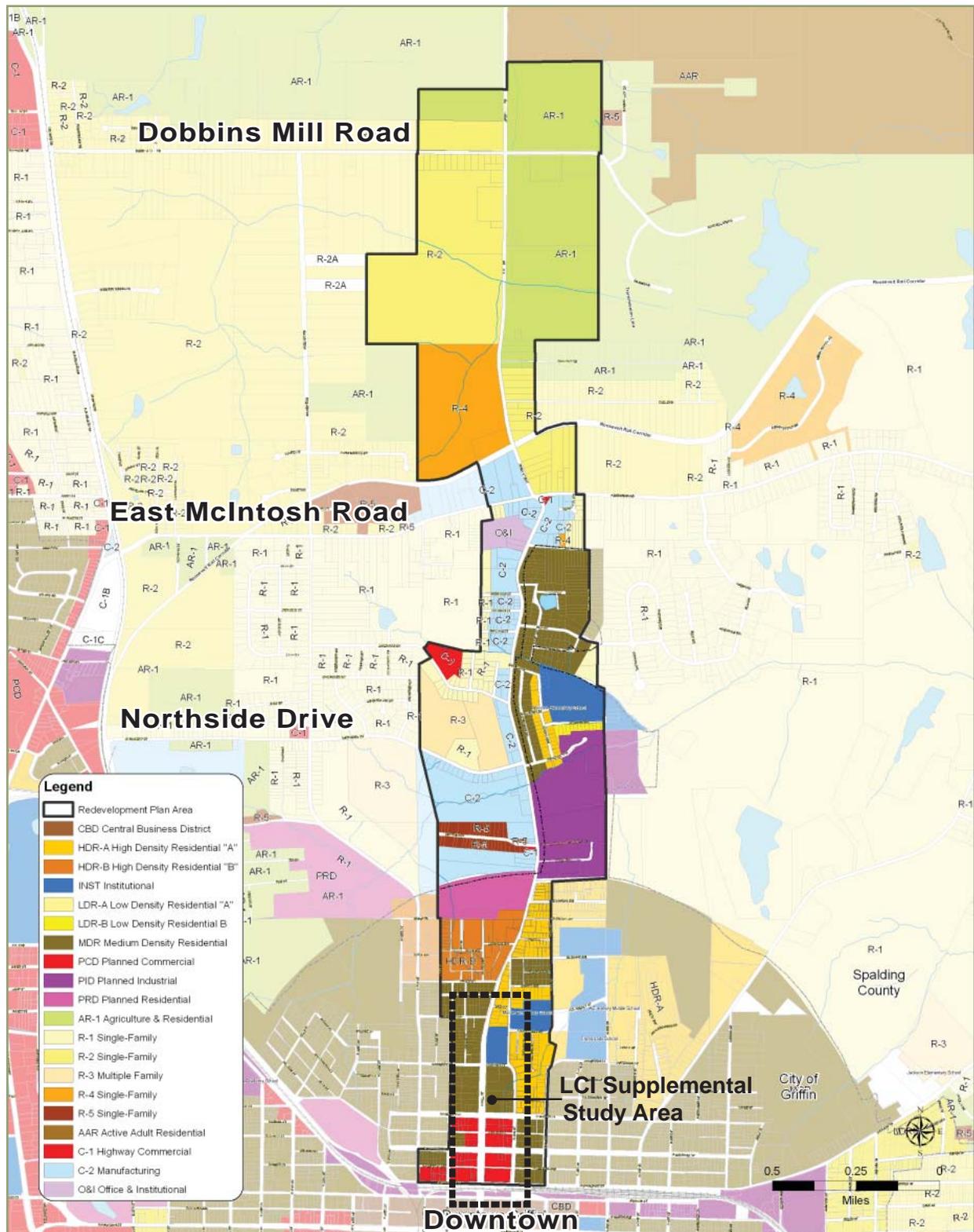


The current zoning has no design requirements



Woodstock, GA, uses zoning to encourage compatible new development

Figure 4: Existing Zoning



III. Environment & Open Space Conditions

Natural Features

An understanding of the interplay between human and natural features is essential to planning for the future. Natural features can define if and how land should be developed, while human activities can have a profound impact on the environment.

Throughout the area, gently rolling topography and pleasant tree coverage dominates the land north of Downtown. The southernmost portion of the Study Area has only a few interactions with streams forming on each side of North Hill Street, within the City of Griffin. Towards the middle portion of the Study Area, North Hill Street dips in elevation and two streams cross underneath it just north and south of Atkinson Elementary School. In these areas, homes are located close to waterways. The north end of North Hill Street also has considerable streams and floodplains that stretch west from Dundee Lake Park.

Strengths

- Connection of streams and bodies of water, which ensures links for hydrology system

Weaknesses

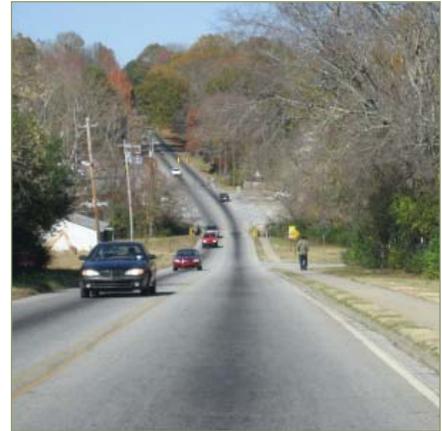
- Existing homes adjacent to floodplains, which face potential flooding problems
- Large areas of parking, which lack trees or landscaping and encourage water runoff

Opportunities

- Streams and floodplains, which offer potential for creation of park land and recreational amenities
- New development, which could positively incorporate stormwater management and open space
- Gently rolling topography, which can support innovative neighborhood design

Threats

- New development, which could occur without usable open space



Gently rolling topography helps to strengthen sense of place and increase visual appeal



Existing land with tree coverage faces potential for change



Large expanses of paved areas send stormwater into nearby streams without adequate natural filtration

Parks and Open Space

Reliance on technology and fast-paced lifestyles often lead people to increasingly value places that allow them to connect with others. In fact, one of today's hottest real estate trends is the community where people can partake in a wide variety of public spaces on a daily basis. Many people are no longer content to drive long distances to walk on a pleasant, tree-lined sidewalk, play in a park with their children, or relax on a warm summer evening. They now want their communities to provide these opportunities and more.

There are five major categories of public spaces, each with their own distinct definition and applicability:

Streets and sidewalks are the most often used public spaces in towns and cities. In addition to serving as a transportation conduit, streets and sidewalks can be designed to encourage socialization and community building. Streets can serve as parade routes or the location of special festivals, while in-town sidewalks can provide room for cafe dining, street furniture, and street trees.

Plazas are hardscaped gathering spaces surrounded by commercial, mixed-use, or civic buildings. Plazas often include fountains, benches, and similar elements. Their entire surface is accessible to the public and consists of stone, concrete, or durable pavement interspersed with trees and limited planting.

Parks are landscaped recreation and gathering places that can be located in any area of a town or city. They may be surrounded by residential or commercial buildings, and are often the focal points of neighborhoods. Parks often include picnic facilities, drinking fountains, benches, and playgrounds. Larger parks may include ponds, sports fields, or courts. Well designed parks are defined at the edges by streets. Their accessible landscape consists of paths, trees, lawns, shrubs, and other plant materials.

Greenways are linear parks that can serve as corridors for transportation, wildlife, or protection of key habitats. They are often found along creeks and rivers, flood zones, or utility corridors. Greenways can also connect plazas, parks and conservation lands. As such, they can be found in any setting with varying sizes.

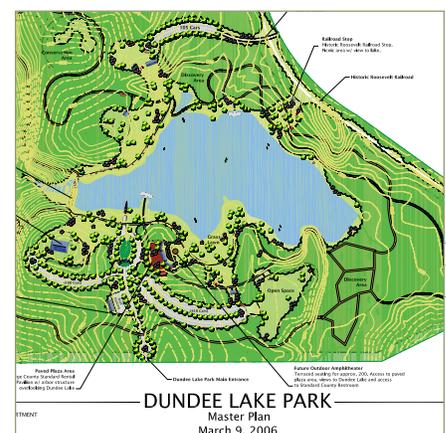
Conservation Lands protect and enhance areas of environmental and historic significance. They are usually located at the edge of a village, town or city. Because their primary purpose is the protection of open space, they can include camping sites and trails.



Disconnected sidewalks fail to meet the needs of pedestrians



Plazas can provide gathering or resting places



Park development plans near the corridor will improve pedestrian and recreational amenities

There are few public open spaces in the Study Area. The most notable one is Dundee Lake Park, just east of the Study's Areas north end. The southern portion of the Study Area has two parks just outside of it: Fairmont Park to the east and Thomaston Mill Park to the west. Undeveloped land along the corridor also has potential for new park space, and could offer amenities to residents and visitors of the North Hill Street corridor.

Strengths

- Improvements to Dundee Lake Park, which will provide an open space anchor to the corridor's north end
- Other existing parks, which offer recreational amenities
- Wide landscape verge at the corridor's southern end, which creates a green streetscape

Weaknesses

- The lack of direct connection along corridor to quality open space, which results in a disconnected system
- Lack of neighborhood parks, which means many people must drive to access open spaces

Opportunities

- New development, which could include open spaces and fill in sidewalk gaps
- Flood zones, which could connect other open spaces and provide trail opportunities

Threats

- Without quality open space along the corridor, residents and visitors will miss many opportunities currently enjoyed in neighboring growth areas

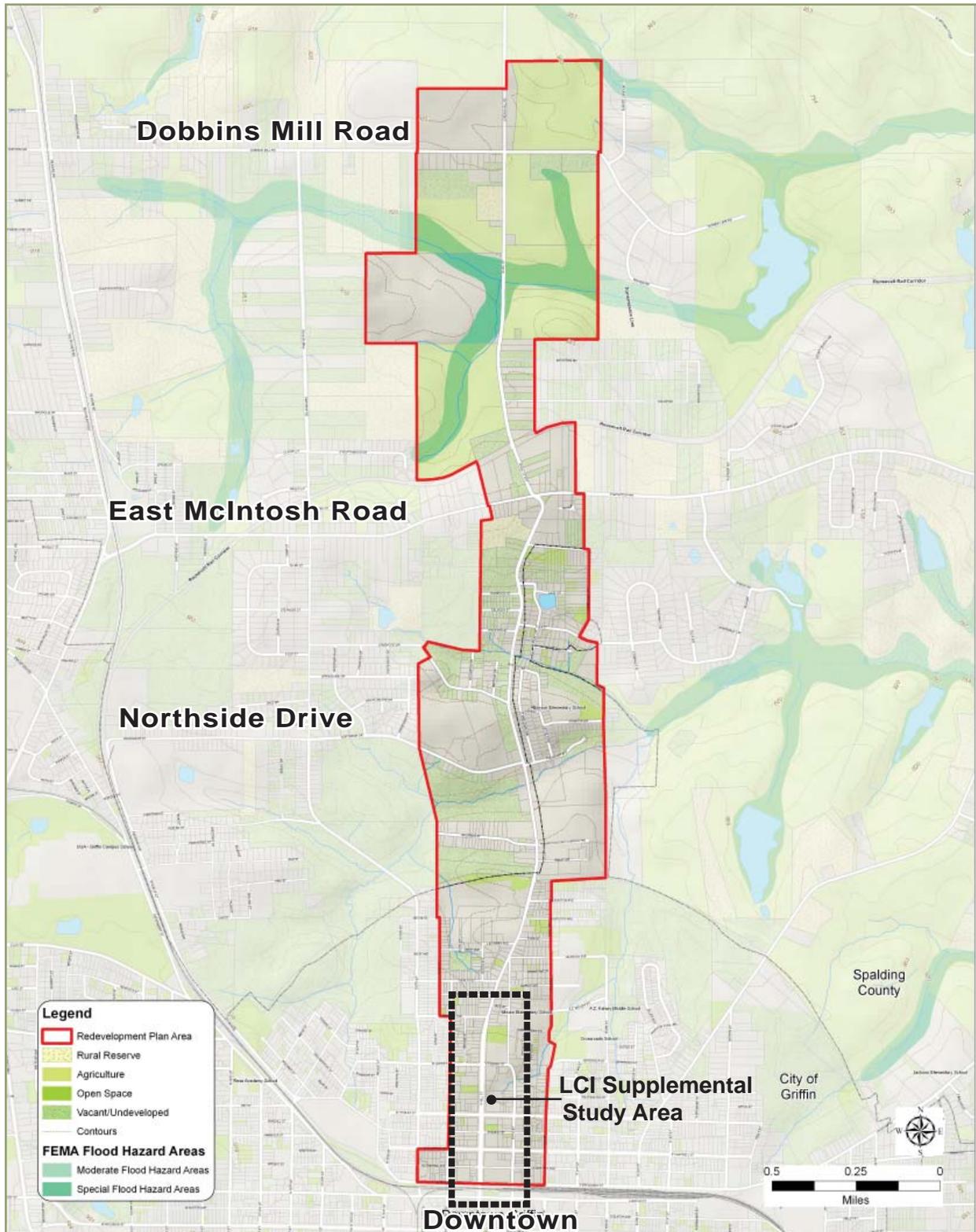


New sidewalks can link residences, businesses, and parks and open spaces



This linear park space offers amenities to the entire community and is located alongside a street

Figure 5: Existing Open Space



IV. Transportation Conditions

Roadway System

The roadway system provides the backbone of any transportation system. To provide a basis for planning, it is necessary to understand existing roadway characteristics and utilization as well as identify issues and needs. The following provides an overview of major roadway characteristics, utilization and safety.

North Hill Street Corridor Roadway Characteristics

The North Hill Street Corridor provides an essential north-south connection between the City of Griffin and north Spalding County as it is the only continual street from the City of Griffin to the Henry County line. Between East McIntosh Road and Dobbins Mill Road, the road name changes to the North Hill Street Extension; at Dobbins Mill Road, the road name changes to Jordan Hill Road. Other nearby roads that provide some parallel north-south connectivity include North 9th Street, west of the Study Area which ends at Dobbins Mill Road and North 2nd Street, east of the Study Area, which terminates north of East McIntosh Road. Major cross-streets within the Study Area include East/West Broadway Street, Northside Drive, East McIntosh Road, and Dobbins Mill Road.

The southern end of the Study Area, within the City of Griffin, is characterized by a well-connected street grid from East/West Broadway Street to East/West Tinsley Street, with block lengths averaging 300 to 500 feet. North of East/West Tinsley Street, the block sizes become irregular and many cross-streets are dead-end streets. North of East McIntosh Road, there is very little street network.

North Hill Street is predominately a two-lane, undivided road, except for a short section close to downtown Griffin between Broadway Street and Chappell Street, where it is a four-lane, median-divided road with on-street parallel parking, as shown in Figure 6. All of the remaining roads within the Study Area are two-lane roads.

The posted speed on North Hill Street ranges from 25 miles per hour (mph) from East/West Broadway Street to Lynn Street to 35 mph from Lynn Street to East McIntosh Road to 45 mph on the North Hill Street Extension and Jordan Hill Road. Most streets within the Study Area have posted speeds of either 25 or 35 mph, as shown in Figure 7.

Figure 8 shows additional roadway characteristics, including functional classification and location of traffic signals and flashing beacons. Understanding functional classification is necessary to properly assess a roadway. Functional classification indicates a roadway's purpose, whether it is primarily for providing vehicular throughput or access. Functional classification is a hierarchical system, with arterials providing the greatest mobility and limited access and local roads providing a higher level of access and lower level of mobility. Collector roadways provide a balance between arterials and local roads by providing access for motorists to the larger roadway system but are typically not intended for long-distance travel.

According to the Georgia Department of Transportation (GDOT), North Hill Street is designated an urban minor arterial from East/West Taylor Street to East McIntosh Road where it changes to a rural major collector. East McIntosh Road is designated an urban collector street from Old Atlanta Road to North Hill Street where it becomes a rural major collector. Additional roads designated as urban collector streets

Figure 6: Number of Existing Lanes

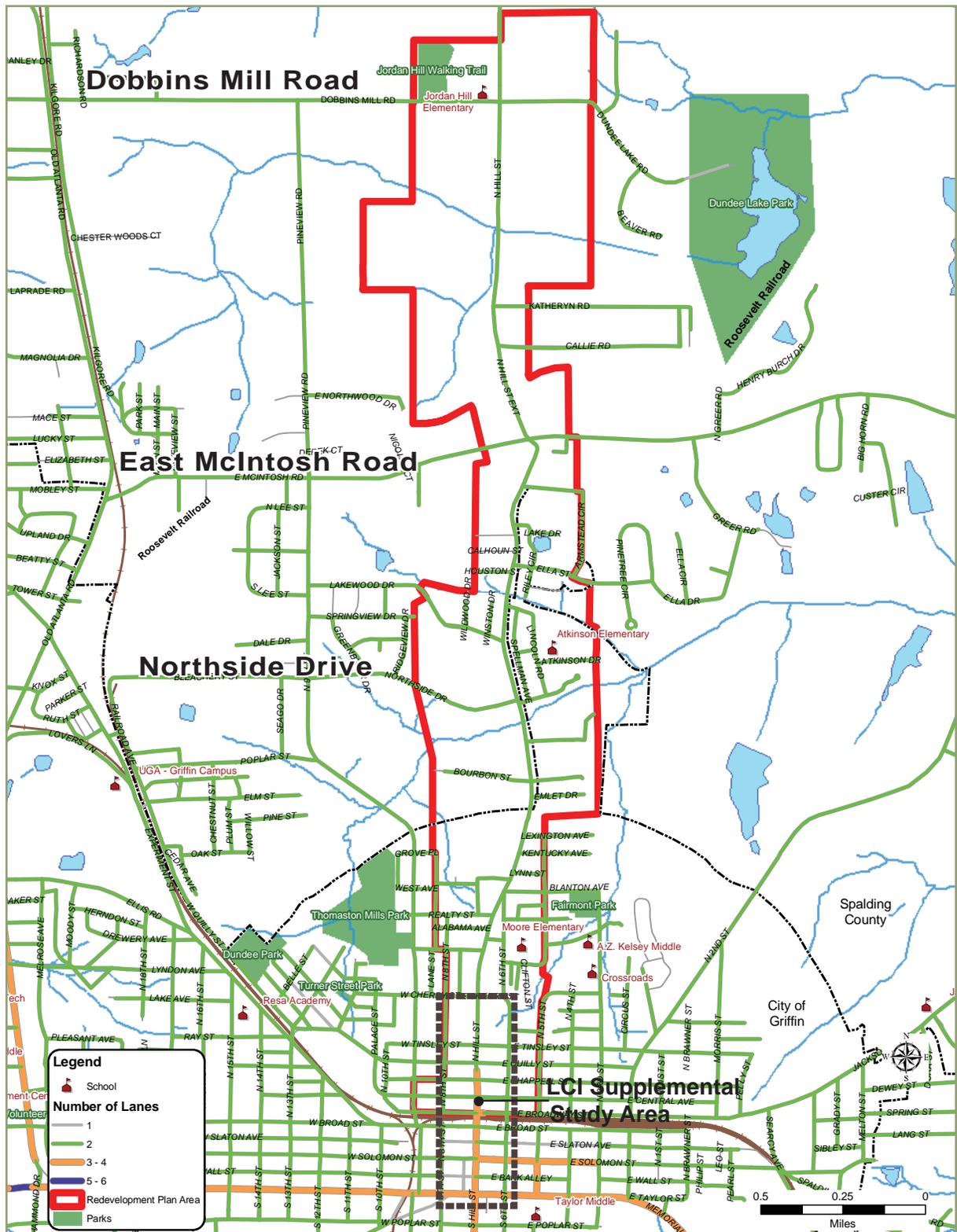
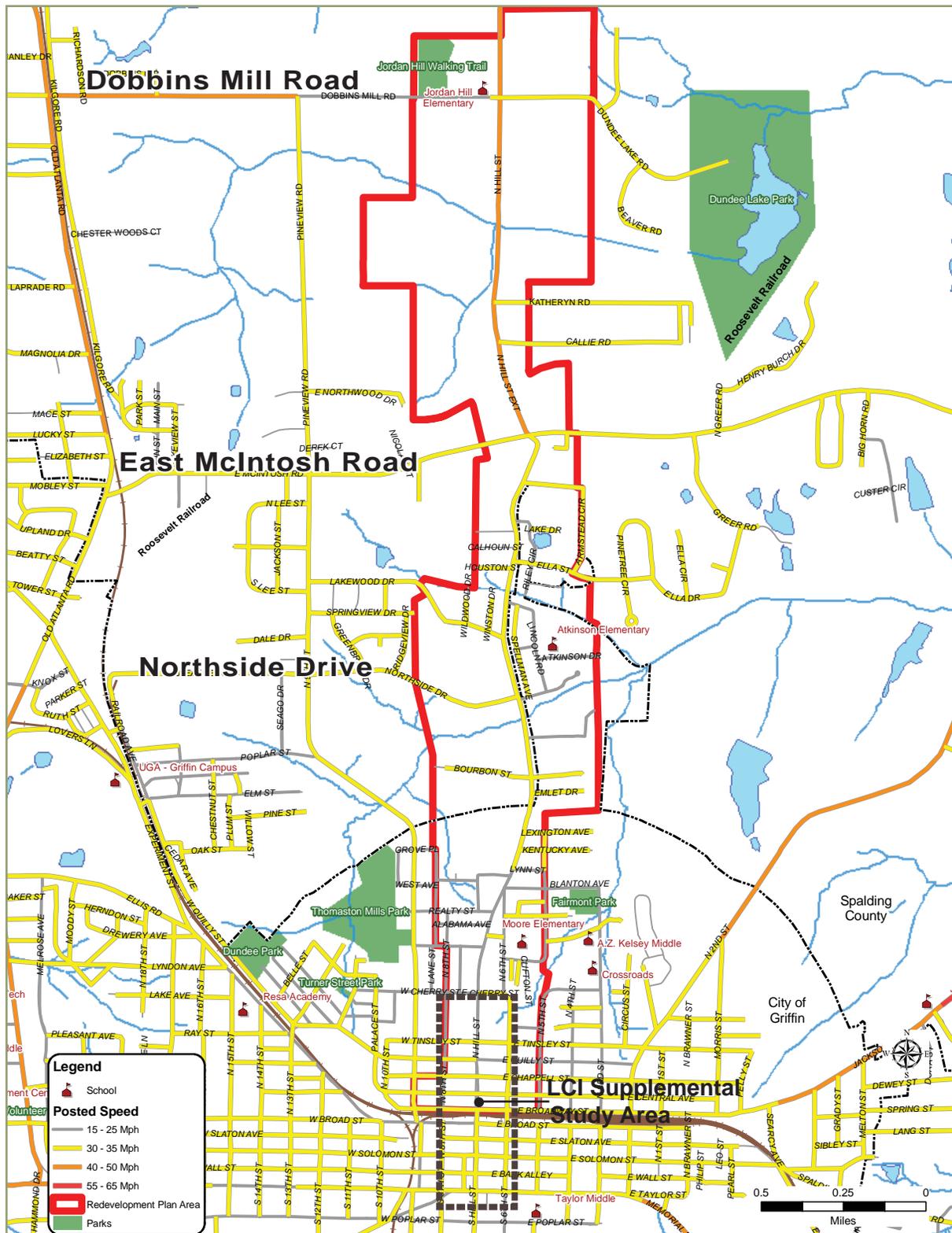


Figure 7: Posted Speeds



include North Sixth Street from East Cherry Street, extending southward to the study boundary, East Cherry Street from North Hill Street to North Sixth Street, and North Ninth Street from West Broad Street to East McIntosh Road. East Broadway Street (SR 155) from North Hill Street east is designated an urban minor arterial. The remaining streets in the Study Area are considered local streets.

Three traffic signals and three flashing beacons are found within the Study Area at the following locations, as illustrated in Figure 8.

Traffic Signals

- North Hill Street at East/West Chappell Street
- North Hill Street at Northside Drive and Tuskegee Avenue
- North Sixth Street at East Chappell Street

Flashing Beacons

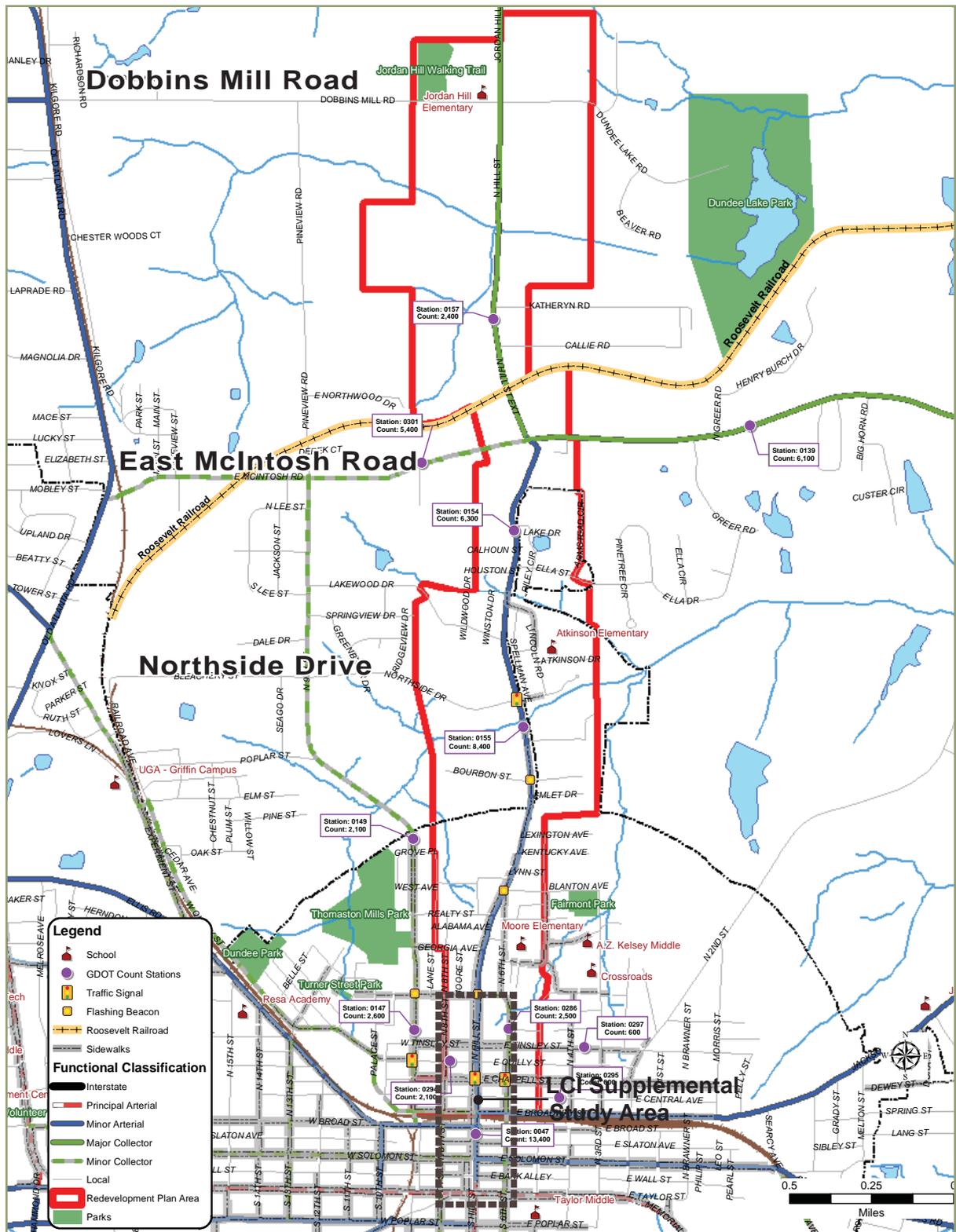
- North Hill Street at East/West Cherry Street
- North Hill Street at North Sixth Street and Blanton Avenue
- North Hill Street at Bourbon Street and Thomas Packing Co. Road

It was observed on a field visit that the signalized intersections do not include provisions for pedestrians, including pedestrian signal heads and push buttons, marked pedestrian cross-walks, and ramps to meet Americans with Disabilities Act (ADA) accessibility requirements. The intersection at North Sixth Street at East Chappell Street only has one signal head for the intersection, which does not meet current guidelines which require redundancy. The intersections at North Hill Street Extension/Jordan Hill Road at Dobbins Mill Road/Dundee Lake Road and North Hill Street at East McIntosh Road are controlled with stop signs.

Related to parking, the Spalding County Code of Ordinances restricts parking on the following streets within the Study Area (Part VII, Chapter 1, Section 7-1004):

- Ella Street/Ella Drive from North Hill Street Extension to Greer Road
- North Hill Street from the city limits of the City of Griffin to East McIntosh Road
- Lakewood Drive from Canterbury Road to North Hill Street

Figure 8: Roadway Conditions



Daily Utilization and Level of Service

Existing traffic volume data was collected from GDOT in and around the Study Area for the years 2004 through 2006, as shown in Table 2 and illustrated in Figure 8. No new traffic counts were conducted for the study. According to the existing data, the greatest daily traffic volumes are found just south of the Study Area on North Hill Street, south of East/West Broad Street. Within the Study Area, the daily traffic volume on North Hill Street ranges from 8,400 vehicles per day (vpd), south of Northside Drive to 6,300 vpd, south of East McIntosh Road. North of East McIntosh Road, the volume on North Hill Street drops considerably to 2,400 vpd.

Table 2: Study Area Annual Average Daily Traffic (AADT)

Traffic Count ID	Street Name	Route Number	From	To	3-year Average AADT (2004-2006)
0047	N. Hill Street	SR 155	E. Taylor Street	E. Broad Street	13,400
0154	N. Hill Street	CR 134	E. Broadway Street	W. Tinsley Street	6,300
0155	N. Hill Street	CR 134	E. Cherry Street	Armstead Circle	8,400
0157	N. Hill Street Extension	CR 134	E. McIntosh Road	Kathryn Road	2,400
0160	Jordan Hill Road	CR 134	Dundee Lake Road	N. 2nd Street Ext.	1,300
0301	E. McIntosh Road	CR 051	N. 9th Street	No Name	5,400
0139	E. McIntosh Road	CR 051	N. Hill Street	Big Horn Road	6,100
0147	N. 9th Street	CR 289	N. 9th Street	W. Cherry Street	2,600
0149	N. 9th Street	CR 289	Georgia Avenue	Poplar Street	2,100
0286	N. 6th Street	CS 797	E. Central Avenue	N. 12th Street	2,500
0294	Quilly Street	CS 616	W. Quilly Street	N. 2nd Street	2,100
0295	E. Central Street	CS 619	N. 1st Street	W. Quilly Street	600
0297	E. Tinsley Street	CS 605	Pelly Street	N. 9th Street	600

Source: GDOT

Existing and future level of service (LOS) for North Hill Street and East McIntosh Road is shown in Table 3. LOS is a qualitative assessment of traffic congestion, ranging on a scale from A to F, where A represents the best condition (little traffic) and F represents the worst condition (highly congested, stop and go traffic).

To estimate future volumes and identify existing and future LOS, the following methodology was employed. Using the Atlanta Regional Commission (ARC) Travel Demand Model, Study Area count locations were reviewed for year 2005 and 2030 to identify the forecasted change in daily traffic over that time period. The forecasted rates of change from the model were then applied to the existing volumes to estimate a future volume at the specific count locations. Existing and future LOS was then identified for each count location based on the Florida Department of Transportation (FDOT) generalized LOS tables for areas transitioning into urbanized areas (Table 4-2). As shown in Table 3, the existing and future LOS is generally acceptable at LOS C or D.

Table 3: Existing and Future Level of Service (LOS)

Traffic Count ID	Street Name	From	To	Average AADT (2004-2006)	LOS	2030 Estimate	LOS
0047	N. Hill Street	E. Taylor Street	E. Broad Street	13,400	C	20,000	D
0154	N. Hill Street	E. Broadway Street	W. Tinsley Street	6,300	C	8,900	D
0155	N. Hill Street	E. Cherry Street	Armstead Circle	8,400	D	10,600	D
0157	N. Hill Street Extension	E. McIntosh Road	Kathryn Road	2,400	*	3,800	*
0301	E. McIntosh Road	N. 9th Street	No Name	5,400	C	7,700	D
0139	E. McIntosh Road	N. Hill Street	Big Horn Road	6,100	C	9,800	D

Source: GDOT, FDOT's Quality/Level of Service Handbook, Table 4-2: *Generalized Annual Average Daily Volumes for Florida's Areas Transitioning into Urbanized Areas or Areas over 5,000 not in Urbanized Areas.*

*Note: The LOS is equal to or greater than LOS C, but cannot be determined based on Table 4-2.

Traffic Safety

Traffic crash data was obtained from GDOT for the years 2004 through 2006 for the Study Area. Over the three-year period, the Study Area experienced, on average, 74 crashes per year. The greatest number of crashes occurred in 2005 (76 crashes) and the least occurred in 2004 (71 crashes). Over the three-year period, seven crashes involved striking a pedestrian and one crash involved a bicyclist (pedalcycle). Two fatal crashes have occurred between 2004 and 2006, accounting for 0.9 percent of all crashes. Nearly 37 percent of all crashes resulted in an injury, and the remaining 62 percent of crashes were property-damage-only crashes. A comparison of the Study Area data to statewide data appear to indicate a greater prevalence for fatal and injury crashes within the Study Area. The Georgia Statewide Mileage, Travel and Accident Data summary for 2004 shows that statewide, 0.43 percent of all crashes were fatal crashes and 26.2 percent of all crashes resulted in an injury.

The first harmful event reported with the greatest frequency was vehicles crashing with other vehicles, accounting for 73.3 percent of all crashes. After hitting another vehicle, the first harmful event reported with the greatest frequency was hitting a parked motor vehicle (6.3 percent), hitting a utility pole (3.2 percent), hitting a pedestrian (3.2 percent), or striking an "other fixed object" (2.3 percent). All other first harmful events identified occurred equal to or less than two percent of the time. The most frequently reported manner of collision was angle crashes (39.8 percent), followed by rear-end crashes (24.4 percent) and collisions with objects other than motor vehicles (19.5 percent).

Within the Study Area, 57 percent of all collisions occurred at intersections. The greatest number of crashes at intersections on North Hill Street occurred at East/West Chappell Street, as shown in Table 3. Input from stakeholders indicated safety concerns at the North Hill Street at Blanton Avenue/North Sixth

Street intersection, the North Hill Street at Northside Drive/Tuskegee Avenue intersection and the North Hill Street at East McIntosh Road intersection. The data show relatively few crashes occurring at these intersections.

Table 3: Total Crashes at North Hill Street Intersections

Intersection	2004	2005	2006	Total
North Hill Street at W. Chappell Street	3	2	3	8
North Hill Street at W. Tinsley Street	2	1	3	6
North Hill Street at E. Central Avenue		2	3	5
North Hill Street at E. Cherry Street	1	3	1	5
North Hill Street at Blanton Avenue	1	3	1	5
North Hill Street at Northside Drive	2	2	1	5
W. Chappell Street at N. 6th Street	3		1	4
Blanton Avenue at S. 6th Street	3		1	4
North Hill Street at Lynn Lane	1	2	1	4
North Hill Street at Ella Street	2	1	1	4
North Hill Street at Kentucky Avenue	1	1	1	3
North Hill Street at Bourbon Street	1	2		3
North Hill Street at E. McIntosh Road		3		3
North Hill Street at Callie Road	2		1	3
North Hill Street at Tuskegee Avenue	1			1

Source: GDOT

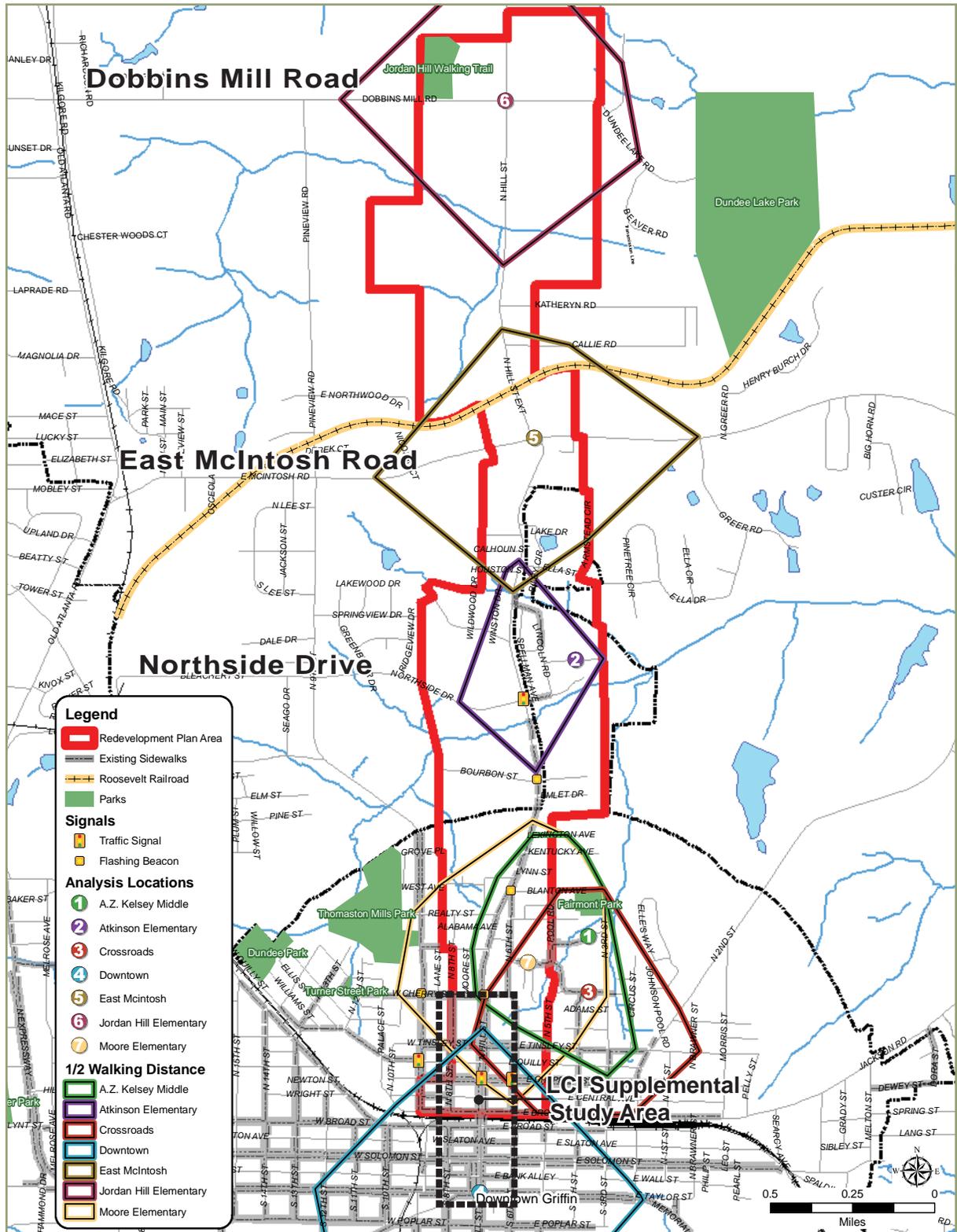
Pedestrian/Bicycle Facilities

An assessment of pedestrian and bicycle facility needs was undertaken by reviewing the inventory of existing facilities as well as conducting a pedestrian facility needs evaluation. Existing pedestrian and bicycle facility data was collected from the City of Griffin and Spalding County and mapped using a Geographic Information System (GIS).

To determine additional pedestrian facility needs, a GIS assessment was used to identify one-half mile walking distances to educational facility and commercial activity area destinations within the Study Area. The seven destinations included five schools (Jordan Hill Elementary School, Atkinson Elementary School, Moore Elementary School, A.Z. Kelsey Middle School, and Crossroads School) and two commercial activity areas (downtown Griffin and East McIntosh Road intersection). Schools were included in the assessment because of the proximity of the schools to adjacent residential areas. In addition, nation and statewide initiatives to promote Safe Routes to School recognize the need to provide safe pedestrian facilities for students to walk to school in grades Kindergarten through the eighth grade.

The existing sidewalk inventory and pedestrian facility needs assessment is shown in Figure 9. As shown, the greatest provision of existing sidewalks is found on the southern end of the Study Area, nearest downtown Griffin. North of Tuskegee Avenue, few sidewalks exist. Based on the half-mile walking

Figure 9: Bicycle and Pedestrian Needs



distance assessment, a need for sidewalks is evident on North Hill Street, North Hill Street Extension, East McIntosh Road, Dobbins Mill Road, Dundee Lake Road, Northside Drive and Lincoln Road. Other concerns identified through field review and stakeholder input include the poor condition of existing sidewalks, lack of or heavily worn marked cross-walks at intersections, lack of ADA-accessible ramps at intersections, and lack of pedestrian signal heads at signalized intersections. The presence of worn foot-paths is evident along North Hill Street where no sidewalks are provided.

No existing bicycle facilities were identified within the Study Area. The City of Griffin 2024 Comprehensive Plan identifies North Hill Street as a bicycle route extending from Blanton Avenue southward into the city center. A proposed greenway is shown connecting Dundee Park, Thomaston Mill Park, Fairmont Park, and Jackson Road, generally following the city boundaries. Another proposed greenway is shown following the Roosevelt Railroad line.

Public Transit

The North Hill Street Corridor Study Area is currently served by a rural demand-response transit system that is administered by the McIntosh Trail Regional Development Center. No fixed-route or commuter service is provided. The closest urban service is the commuter Xpress Bus service operated by the Georgia Regional Transportation Authority (GRTA) on US 19/41. Xpress Bus Route 440 operates weekdays during the morning and afternoon commute periods and provides service to and from downtown Atlanta from the Hampton Motor Speedway in Henry County and the Clayton County Justice Center which are located north of the Study Area.

Study area demographic characteristics for 2007 were reviewed to identify potential transit needs. The source of data was Claritas, Inc., and the data represents statistical estimates rather than actual tabulations. Populations that are more likely to need or use public transit include non-white persons, youth, elderly, those without access to vehicles and low-income persons. Table 5 shows a summary of the following characteristics: non-white persons, persons age 10 to 17, and persons age 65 and over, while Table 6 shows households without vehicles, households with income levels below \$25,000, and household median income. The Study Area data are compared to those of Griffin and Spalding County.

The Study Area appears to have much greater concentrations of potential transit markets than is found in either the City of Griffin or Spalding County. The Study Area has a much greater proportion of non-white persons at 80.6 percent than is found in Griffin (56.5 percent) or Spalding County (35.7 percent). The median income level in the Study Area is considerably lower at \$27,455 than is found in Griffin

Table 5: Transit Market Population Characteristics - 2007

Geography	Total Population	Non-white Persons		Persons Age 10-17		Persons Age 65+	
		Total	Percent	Total	Percent	Total	Percent
Study Area	2,849	2,297	80.6%	373	13.1%	290	10.2%
City of Griffin	24,150	13,647	56.5%	2,901	12.0%	3,094	12.8%
Spalding County	62,099	22,176	35.7%	7,163	11.5%	7,387	11.9%

Source: Claritas, Inc.

(\$40,345) or Spalding County (\$48,777). Over one-quarter of households within the Study Area lack a vehicle, compared to 18.9 percent of households in Griffin and 10.1 percent in the county. A slightly larger proportion of persons age 10 to 17 live within the Study Area at 13.1 percent than is found in Griffin (12 percent) or Spalding County (11.5 percent). A slightly smaller proportion of persons age 65 and over live within the Study Area at 10.2 percent than is found in Griffin (12.8 percent) or Spalding County (11.9 percent).

Table 6: Transit Market Household Characteristics - 2007

Geography	Total Households	Households Lacking Vehicles		Household Income below \$25,000		Median Household Income
		Total	Percent	Total	Percent	
Study Area	1,001	251	25.1%	329	46.3%	\$27,455
City of Griffin	9,231	1,742	18.9%	1,845	29.9%	\$40,345
Spalding County	23,193	2,337	10.1%	3,751	22.1%	\$48,777

Source: Claritas, Inc.

Rail

The North Hill Street Corridor Study Area begins just north of a major at-grade rail crossing between East/West Broad Street and East/West Broadway Street on a Norfolk Southern Rail line (ID number 718195C). According to the Federal Railroad Administration (FRA) this crossing averages ten trains per day. Another rail crossing is identified north of East McIntosh Road on North Hill Street Extension (ID number 718718E), though the rails are not evident. This crossing is on a line where service has been discontinued. A portion of this rail line is owned by the Roosevelt Railroad, a tourist operation.

Currently Planned Projects

In order to address needs within the Study Area, it is important to review what transportation improvements already are programmed or planned for the area. Local, regional and state transportation plans were collected and reviewed to identify projects within the area. Plans reviewed include:

- ARC Envision6 Regional Transportation Plan (RTP) and 2008-2013 Transportation Improvement Program (TIP)
- GDOT Construction Work Program (CWP)
- McIntosh Trail Regional Bicycle and Pedestrian Pathway Plan (April 2005)
- Spalding County 2024 Comprehensive Plan (2004)
- City of Griffin 2024 Comprehensive Plan
- Spalding County Comprehensive Transportation Plan, Assessment of Existing Transportation Conditions (January 2007)
- Dundee Lake Park Master Plan Map (March 2006)
- Griffin Town Center Livable Centers Initiative Study (LCI) (November 2006)

Programmed and planned projects included in the ARC Envision6 RTP and the GDOT CWP in and around the Study Area are shown in Table 7. Only two projects are located within the Study Area: SP-070, a feasibility study for Southern Railway corridor multi-use facility (which includes the Roosevelt Railroad line) and SP-071, a bridge upgrade on North Hill Street at Cabin Creek. Other projects which have been identified within the Study Area from other plans are summarized in Table 8.

Table 7: Planned and Programmed Projects included in ARC and GDOT Plans

ARC Project Number	GDOT PI Number	Location	Description	Project Type	TIP/RTP Status	Sponsor	Location	Length (miles)	Model Network Year	Total Cost
SP-021	332890	US 19 at SR 16	US 19 Turn Lanes at SR16	Roadway Operational Upgrades	Programmed	GDOT	Spalding County	.33	2020	\$4,549,000
SP-022	342621	US 19/ SR 3	Widen from 4 to 6 lanes from north of West Ellis Rd. to north of Laprade Rd.	General Purpose Roadway Capacity	Long Range	GDOT	Spalding County	2.2	2020	\$10,627,000
SP-027	343460	Sixth Street	Bridge Upgrade at SR 155 and Norfolk Southern rail line	Bridge Upgrade	Programmed	GDOT	Spalding County	.2	2020	\$2,710,000
SP-068A	0008235	Various locations in the city of Griffin	Signal Upgrades	Roadway Operational Upgrades	Programmed	City of Griffin	Spalding County	N/A	2010	\$1,130,000
SP-068B	0008236	Various locations in the city of Griffin	Signal Upgrades	Roadway Operational Upgrades	Programmed	City of Griffin	Spalding County	N/A	2010	\$418,000
	331720	Bridge Upgrade	CR 508 /Jordan Hill Road north of SR 16 @ Troublesome Creek	Temporarily Shored Bridges		GDOT Let	Spalding County	N/A		\$179,000
SP-069A	0008237	Various locations in the city of Griffin	Intersection Improvements	Roadway Operational Upgrades	Programmed	City of Griffin	Spalding County	N/A	2010	\$1,775,000
SP-069B	0008238	Various locations in the city of Griffin	Intersection Improvements	Roadway Operational Upgrades	Programmed	City of Griffin	Spalding County	N/A	2020	\$1,950,000
SP-070	0008239	Southern Railway, Spalding County	Feasibility study for Southern Railway corridor multi-use facility	Studies	Programmed	Spalding County	Spalding County	N/A	2010	\$152,000
SP-071	0006954	North Hill Street at Cabin Creek	North Hill Street bridge upgrade	Bridge Upgrade	Programmed	GDOT	Spalding County	N/A	2020	\$562,000
SO-166	0000294	US 19/41 Herman Talmadge Highway	From Laprade Road in Spalding County to SR 20 (Richard Petty Boulevard/ Woolsey Road) in Henry County	General Purpose Roadway Capacity	Programmed	GDOT	Multi County, including Spalding	8.5	2020	\$48,476,000

Source: ARC Envision6 Regional Transportation Plan (RTP) and 2008-2013 Transportation Improvement Program (TIP), GDOT Construction Work Program (CWP)

Table 8: Other Planned Improvements

Project Description	Project Type	Cost (if available)	Year	Source
Sun City to Downtown Connectivity Study	Study	\$60,000	2007	<i>Griffin Town Center LCI</i>
North Hill Street Pedestrian Facilities, Taylor Street to Tinsley Street	Pedestrian	\$1,226,396	2010	<i>Griffin Town Center LCI</i>
North 9th Street Sidewalks - Georgia Avenue to Broadway Street, 6 foot-wide, west side, 2,600 feet	Roadway Operations	\$107,640	2012	<i>Griffin Town Center LCI</i>
Signal Upgrades, Hill Street at Chappell Street (included in 12 intersection upgrade plan to meet MUTCD standards)	Roadway Operations	\$960,000	n/a	<i>Griffin Comprehensive Plan, Capital Improvement Program, Short-Term Project</i>
Signal Upgrades, Hill Street at Northside Drive, Cherry Street at N. 6th Street, E. Chappell Street at N. 6th Street (included in 12 intersection upgrade plan to meet MUTCD standards)	Roadway Operations	\$960,000	n/a	<i>Griffin Comprehensive Plan, Identified Needs</i>
Intersection improvements: N. Hill Street at Ella Street, North Hill Street at Lincoln Road (included in 6 minor intersection improvements)	Roadway Operations	\$38,000	n/a	<i>Griffin Comprehensive Plan, Capital Improvement Program, Short-Term Project</i>
Intersection Improvements N. Hill Street at East/West Broadway Street	Roadway Operations	\$200,000	n/a	<i>Griffin Comprehensive Plan, Capital Improvement Program, Intermediate-Term Project</i>
Hill Street Widening	Roadway capacity	\$2,900,000	n/a	<i>Griffin Comprehensive Plan, Capital Improvement Program, Intermediate-Term Project</i>
N. Hill Street at N. 6th Street intersection improvement	Roadway Operations	\$500,000	n/a	<i>Griffin Comprehensive Plan, Capital Improvement Program, Long-Term Project</i>
N. Hill Street at Northside Drive intersection improvement	Roadway Operations	\$500,000	n/a	<i>Griffin Comprehensive Plan, Capital Improvement Program, Long-Term Project</i>

n/a: not available

Summary of Issues and Needs

Overall, the North Hill Street Corridor Study Area currently experiences relatively low to moderate traffic volumes. Although traffic is expected to increase on North Hill Street as development occurs north of Study Area, it is not anticipated that the volumes will result in unacceptable LOS. Particular roadway needs include limited connectivity of Study Area roads to areas east and west and roadway design concerns. The current cross-section of North Hill Street is inconsistent from south to north, from an urban section with sidewalks, curbs, and gutters to rural section with soft shoulders, ditches and lack of sidewalks. A number of intersections along North Hill Street have been identified by stakeholders and the public as safety concerns, due in part to sight-distance and alignment problems. Traffic control devices and signage are dated.

The Study Area experiences heavy pedestrian demand, and pedestrian infrastructure is inconsistent throughout the corridor. Although the sidewalk network is extensive on the southern end of the corridor, some of the sidewalks are in need of repair or replacement. High visibility cross-walks, accessible curb ramps, and pedestrian signals are lacking. Very little pedestrian infrastructure is provided north of Tuskegee Avenue to connect residential areas to commercial nodes, schools or recreational facilities. No marked bicycle facilities are present throughout the Study Area.

V. Public Participation

The primary method of public input into the planning process was a Community Workshop held on Saturday, February 9, 2008, at the Griffin Regional Welcome Center. The workshop began with an introduction and welcome followed by an overview of existing conditions, including study purpose and Study Area boundaries. The overview of inventory work focused on three main areas: Land Use and Design; Transportation; and Economics and Markets.

Following the overview, participants broke into small groups around worktables with base maps and supplies. Each table created their desired vision for the future of the North Hill Street corridor. Following the breakout session, participants regrouped and presented their ideas and discussions to the larger group in attendance. Major topics included: preservation areas; identification of activity centers; land use; and transportation.

Following the presentations, next steps were outlined. These included studying and synthesizing ideas presented; establishing preliminary recommendations; finalizing recommendations; and preparing graphic renderings of desired concepts. Questions and answers followed, and the meeting ended.

Limited interviews of key stakeholders were also conducted. The outcomes of these and the workshop have guided the recommendations contained in Sections VI.



Existing conditions were presented at the start of the meeting to familiarize everyone with the Study Area



Work tables allowed participants to illustrate their preferences for the future of the corridor in small groups



Table leaders presented their group's ideas to the larger audience. The sheets had specific recommendations and ideas grouped into Preservation Areas, Activity Centers, Land Use, and Transportation

VI. Recommendations

Vision

This section includes recommendations for the greater Study Area. These recommendations define its future character and provide short and long-range actions to improve the challenges identified during the planning process.

Recommendations are a synthesis of the desires of residents, businesses, property owners, the City of Griffin, Spalding County, and other stakeholders, coupled with sound planning. They offer a visionary yet achievable blueprint for improving transportation connectivity between SunCity Peachtree and Downtown Griffin, and establishing a land use vision that is integrated into this transportation goal. To this end, they enhance the transportation and land use relationship by:

- Directing growth into the corridor
- Establishing pedestrian-oriented mixed-use centers that support walking
- Encouraging redevelopment of marginal land uses
- Connecting existing and new centers with multi-modal transportation enhancements, including new sidewalks, multi-use trails, bike lanes, and vehicular upgrades
- Utilizing open spaces and tree-lined streets to link destinations and improve recreational options

With time, these recommendations will guide growth along North Hill Street in a way that improves the corridor for all users and achieves: walkable mixed-use centers; safe, tree-lined sidewalks; preserved historic resources; compatible new development; and a balanced transportation system.

Within this section, recommendations are organized into two major categories: Land Use and Transportation. Within each, there are two possible types of recommendations provided: Policies and Projects. Please see the inset at right for more details on each.

Policies & Projects

There are two types of recommendations provided in this study:

Policies are guidelines that provide direction to the implementation of the study's vision. They often support projects and should be the basis for actions by the city and county.

Projects are specific tasks, such as transportation improvements, studies, or signage, with a defined cost and time frame.



Projects can be public, such as new sidewalks (top) or private, such as redevelopment (bottom)

Land Use Recommendations

Guiding Land Use Principle: Nodes

To ensure that the Study Areas for both the Supplemental Study and the Redevelopment Plan redevelop and grow in a coordinated manner that balances transportation and land use, it is necessary to focus growth into a series of walkable nodes, including:

- Downtown Griffin;
- A small neighborhood commercial node at Blanton Avenue;
- A neighborhood commercial node at Northside Drive/ Tuskegee Avenue;
- A commercial node at East McIntosh Trail Road; and
- A small park-oriented and neighborhood commercial node at Dobbins Mill Road.

Residential areas or open space should be provided between these nodes in order to ensure stable neighborhoods and provide needed amenities. This will serve key community desires, including encourage walking, allowing marginal land uses to redevelop, preserving open space, encouraging job growth, and minimizing curb cuts.

Supplemental Study Area Land Use Policies

- Preserve, protect, and encourage the rehabilitation of historic buildings (those over 50 years old).
- Protect neighborhoods from unplanned commercial and multifamily intrusion.
- Allow small lot detached houses and townhouses in the medium density residential neighborhoods along both side of North Hill Street, provided they are historically compatible.
- Encourage a maximum height of two-to-three stories in all nodes, except Downtown.
- Support mixed-use development on North Hill Street (see Figure 10), but ensure that development in areas with a historic residential character respect that character and build around historic structures, if possible.
- Support mixed-use land uses along Broadway, but with a commercial character, resembling buildings in the core of Griffin or historic mill buildings.
- Encourage former schools to convert to housing or job-creating incubator business space.



Downtown is the largest commercial node along the corridor



Historic homes should be preserved and restored



Historic homes along the corridor could be converted to commercial uses in the proposed mixed-use area

Supplemental Study Area Land Use Projects

- Amend the city’s land use plan to reflect the study’s vision.
Changes should include:
 - *Reclassifying “Neighborhood Business” parcels to “Medium Density Residential.”*
 - *Reclassifying the Moore Elementary School site to “Mixed-Use.”*
- Update the zoning code to reflect the plan’s visions.
Changes should include:
 - *Rezone the proposed Blanton Avenue Commercial node to Planned Commercial District or a similar designation.*
 - *Incorporate the residential and commercial character standards into an overlay district.*
- Convert Moore Elementary School into incubator commercial space and new infill housing.

Residential Character Standards

The following minimum standards are recommended for areas where a residential character is desired. These include buildings housing commercial uses suggested in Figure 10.

Building Placement

- Match the setbacks of adjacent historic homes.
- Encourage the first story to be elevated 2 to 4 feet, unless existing topography is greater or wheelchair access is desired and can’t be provided from the side or rear.
- Do not provide frontal parking pads.

Fences and Walls

- Encourage a fence, wall, curb or hedge between 6 and 32 inches in height at the back of the sidewalk.
- Limit fences adjacent to the street to picket-wood, stone, composite materials or ornamental metal, with the finished side facing the street.
- Limit retaining walls adjacent to the sidewalk to 32 inches in height unless required by topography.
- Face street-facing walls with stone, brick, or stucco.

Building Facades

- Use historic styles of Griffin as models for new buildings.
- Provide doors and vertical windows arranged horizontally.



New buildings should fit in with the Study Area’s historic character

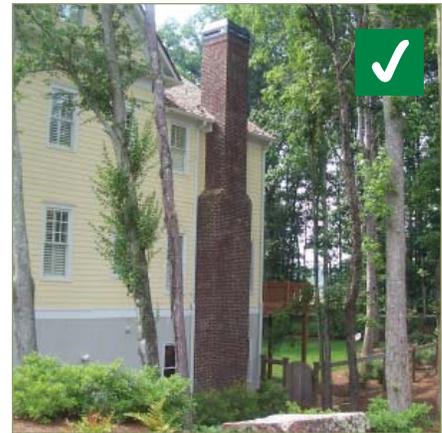


The front yard of the home should not be paved and the shutters and windows should be differently detailed



These new homes in Covington, GA, would be compatible with the Study Area

- Provide windows for a minimum of 20 percent and a maximum of 50 percent of the total sidewalk-level street facade area, with each façade being calculated independently.
- Provide front porches with a minimum depth of 8 feet on the front of all buildings.
- Require stair risers and foundations to be enclosed.
- Prohibit garage doors within 20 feet of the front facade, and discourage them facing the street.
- Prohibit snap-in window muntins or grids-between-glass. Rather, encourage one-over-one windows or simulated divided lights.
- Require shutters to be one-half the width of the window.
- Prohibit chimneys exposed to the public view faced in wood, cement-based, exposed metal, or ceramic pipe. Chimneys should be wrapped in a brick, stone or masonry.
- Require chimneys on exterior building walls to begin at grade and extend between 3 and 6 feet above the roof line.
- Require facade materials to be arranged horizontally, with materials having a more visually-heavy appearance placed below lighter ones. No more than two different materials or colors should be used on one facade.



Chimneys should extend to the ground



Prominent front garages should not be provided

Commercial Character Standards

The following minimum design standards are recommended where a commercial character is desired.

Building Placement

- Place front facades a maximum distance of 5 feet from the back of the sidewalk.
- Provide a first story at-grade with the adjacent sidewalk.
- Prohibit frontal parking.

Fences and Walls

- Prohibit fences or walls between a building and street.

Building Facades

- Provide doors and vertical windows arranged horizontally at the sidewalk-level.
- Provide windows for between 20 and 40 percent of the area of upper story street-facing facades, with each façade calculated independently.



Potential character of a new commercial building

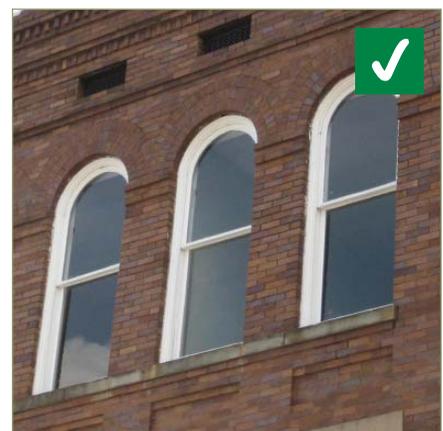
- Provide equally-sized upper story windows in a grid pattern.
- Provide storefronts at the ground level consisting of:
 - A non-glass base or knee wall beginning at grade and extending not more than 24 inches above the sidewalk.
 - A tinted glass window beginning at the top of the bulkhead or knee wall, to a height not less than 10 feet and not more than 12 feet above the adjacent sidewalk. Such glass should provide views into display windows having a minimum depth of two and one-half feet and that are accessible from the building interior.
 - A main entry door remaining unlocked during normal business hours, and having a surface area that is a minimum of 70 percent glass.
 - A glass transom located above the glass display window between 18 and 36 inches in height.
 - A minimum of 75 percent of the length of the first floor facade shall be provided in glass, including glass doors and display windows.
 - No linear distance of more than 10 feet without intervening glass display windows or glass doors.
 - First story drop ceilings recessed a minimum of 18 inches from the display window opening.
- Require stair risers and foundations to be enclosed.
- Prohibit garage doors within 20 feet of the front facade, and discourage them facing the street.
- Prohibit snap-in window muntins or grids-between-glass. Rather, encourage one-over-one windows or simulated divided lights.
- Limit street facing facades to brick, stone, or hard coat stucco.
- Require facade materials to be arranged horizontally, with materials having a more visually-heavy appearance placed below lighter ones. No more than two different materials or colors should be used on one facade.



Upper story windows should be arranged in a grid pattern

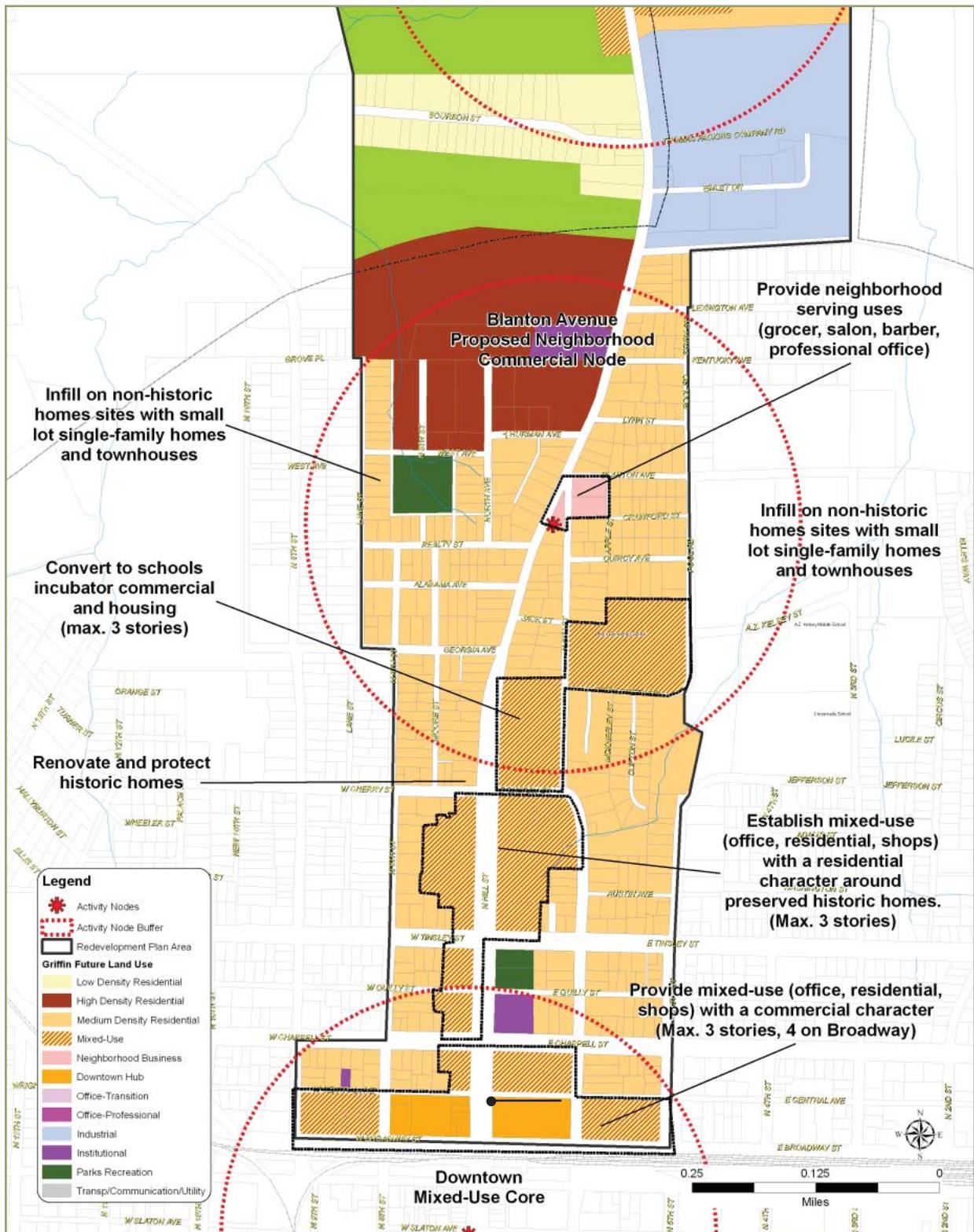


Windows should not be tinted



One-over-one windows should be provided in place of snap-in muntins

Figure 10: Supplemental Study Land Use Recommendations



Transportation Recommendations

Recommendations for the North Hill Street Corridor Study Area have been developed to address identified issues and needs. Recommendations consider what has already been planned and programmed, input from the general public, project stakeholders, and jurisdictions as well as community goals. Transportation goals from the City of Griffin and Spalding County, shown below, help guide identification and selection of projects and policies. Of particular note are goals to increase pedestrian and bicycle facilities and increase accessibility.

Note:
This section reviews transportation alternatives for the entire North Hill Street Corridor. However, only those specific to the supplemental study are recommended by this study.

Transportation Alternatives

Transportation project alternatives include roadway upgrades, intersection realignments, traffic operational improvements, and pedestrian and bicycle facilities. It should be noted that all of the transportation projects are based on planning-level assessment. More detailed traffic or engineering design assessments will be required as project development process continues to support project need and finalize project design. Should projects receive federal funding, they will be subject to environmental review per the National Environmental Policy Act (NEPA). The following provides project descriptions, need, and purpose. Project locations are shown in Figures 11 and 12.

City of Griffin 2024 Comprehensive Plan Transportation Goals

1. Provide safe and efficient vehicular access to and from the city.
2. Develop a network of pedestrian and bicycle trails that support the transportation, recreational, and economic development interests of the community.
3. Support the development of public transit in the form of both commuter rail and local van/bus service.
4. Support the development of one or more bypass routes to alleviate downtown truck traffic.
5. Provide safe and effective local access to aviation facilities in support of local economic development efforts, and reduce the negative impacts of the airport on surrounding residential uses.

Spalding County 2024 Comprehensive Plan Transportation Goals

1. Improve access to and from the county.
2. Improve bicycle and pedestrian ways as a means to offer recreational opportunities for residents and tourists. Support the local rails-to-trails efforts to construct a trail network that would tie in with a larger regional system.
3. Provide sidewalks and multi-purpose paths within and between community centers, thus offering greater mobility and a choice of transportation options.

Figure 11: Alternative Roadway Improvements

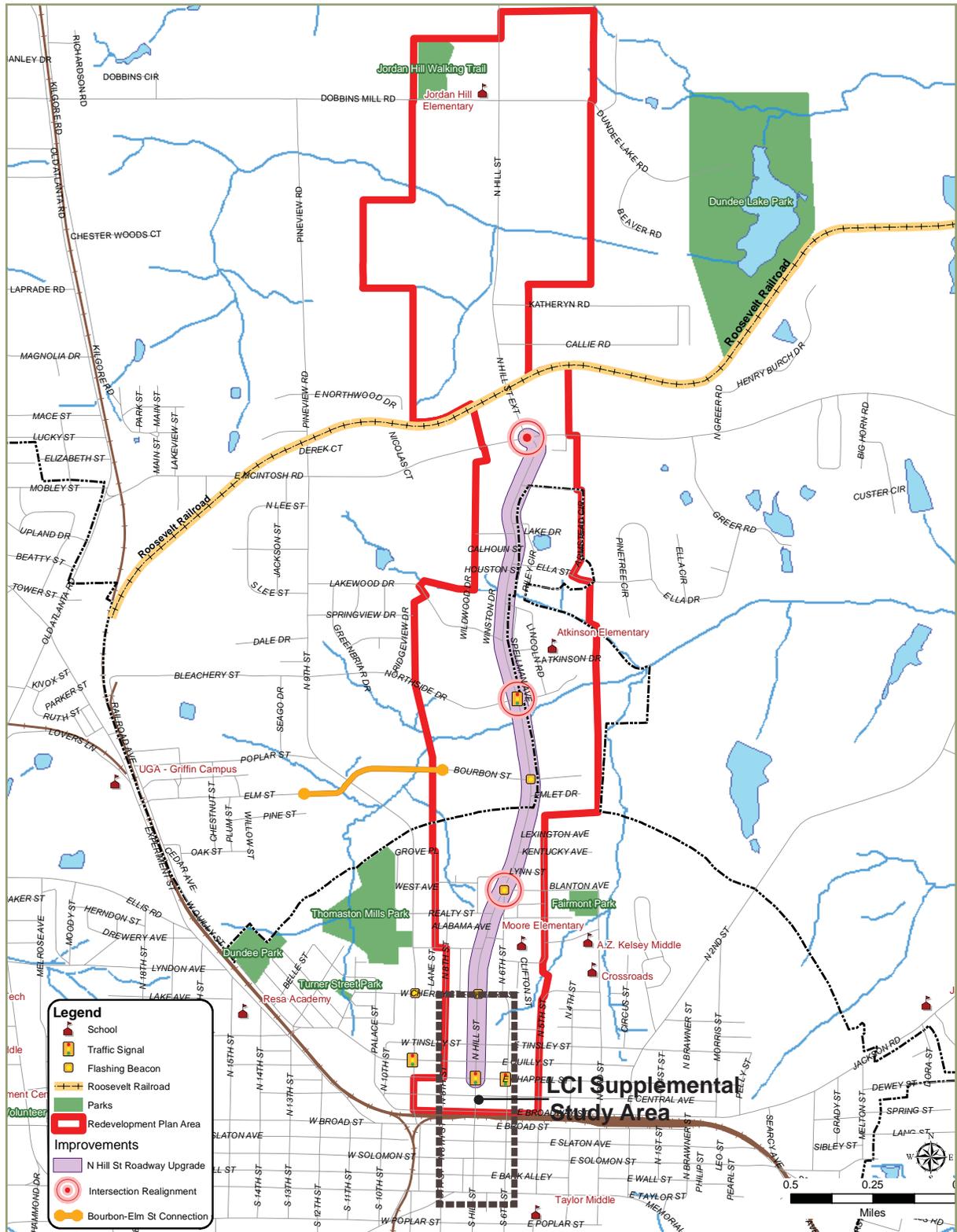
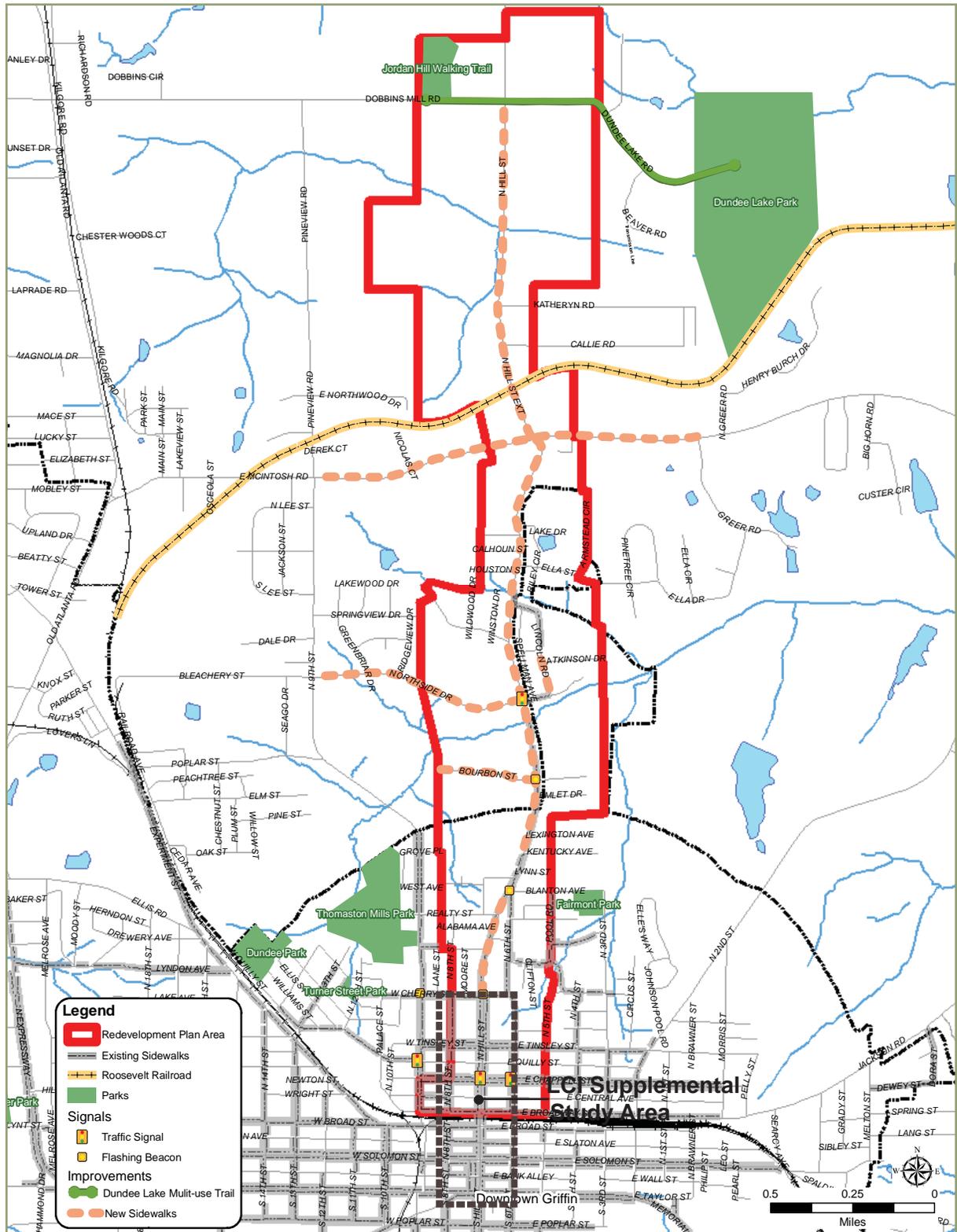


Figure 12: Alternative Bicycle and Pedestrian Improvements



Roadway and Traffic Operational Alternatives in the Supplemental Study Area

The recommended alternative roadway and operational improvements are generally intended to bring the existing system up to current standards for roadway design and traffic operations. Most of the projects also have safety, connectivity and accessibility benefits for the Study Area.

Realign intersection of North Hill Street at Blanton Avenue and North 6th Street to address safety concerns.

The intersection of North Hill Street at Blanton Avenue and North 6th Street has been identified for realignment due to its configuration as an irregular three-way approach intersection, North 6th Street meets North Hill Street and Blanton Avenue at a sharp angle. As a planning level concept, it is recommended that North 6th Street be closed south of Blanton Avenue which will eliminate the three-way intersection. As part of the realignment, Realty Street should be connected across North Hill Street and North 6th Street to Quincy Avenue. An intersection improvement at this location has been previously identified in the City of Griffin 2024 Comprehensive Plan.

Upgrade traffic signs and pavement markings throughout Study Area to current standards.

Throughout the Study Area, it was observed that many traffic warning signs were not standard and pavement markings at intersections were worn or missing. It is recommended that a traffic inventory be conducted and that signs and pavement markings be upgraded to meet current standards.

Upgrade traffic controls for pedestrians at the intersections of North Hill Street and East/West Chappell Street and East Chappell Street at North 6th Street to meet current standards.

Although North Hill Street, East/West Chappell Street and North 6th Street currently have sidewalks on both sides of the street, the crossings at the intersections are lacking pedestrian cross-walks and pedestrian signal heads. Both intersections should be upgraded to current traffic control and pedestrian crossing standards which include ADA accessible curb ramps, high-visibility crosswalks and pavement markings as well as pedestrian signal heads. For planning purposes, it is assumed that the existing hanging traffic signals will be replaced with signals mounted on poles and mast arms similar to what is currently used at the intersection of North Hill Street and East/West Broadway Street. Signal upgrades for these locations have been previously identified in the City of Griffin 2024 Comprehensive Plan.

Upgrade North Hill Street to a consistent cross-section with two through-lanes, curb, gutter, sidewalks and bicycle lanes from East/West Chappell Street to East McIntosh Road.

To provide a consistent character and design and accommodate vehicles, bicyclists, and pedestrians efficiently and safely, it is recommended that North Hill Street be upgraded to include curb and gutter, bicycle lanes, and sidewalks extending from East/West Chappell Street to East McIntosh Road. Currently, North Hill Street has an urban cross-section at the southern end of Study Area, but the cross-section quickly transitions to a rural cross-section. A consistent cross-section will provide a gateway connection to the City of Griffin to the north and better accommodate traffic. Bicycle lanes and sidewalks should be added to increase alternative mode access to schools, commercial areas, and other corridor destinations, consistent with goals established by the City of Griffin and Spalding County. Sidewalks should be set back from the road and separated with a landscape buffer. The North Hill Street improvements include signal upgrades and the realignment of North Hill Street at East McIntosh Road.

Other Roadway and Traffic Operational Alternatives

Realign intersection of North Hill Street at East McIntosh Road to address safety concerns.

To address safety concerns at the North Hill Street and East McIntosh Road intersection, the intersection should be realigned to increase sight distances, straighten North Hill Street and meet current roadway design standards. For planning purposes, it is assumed that sidewalk connections are provided and that the intersection remains a four-way stop. A design-level traffic operational assessment should be conducted to identify the need for turning lanes or determine a signal warrant.

Realign intersection of North Hill Street at Northside Drive and Tuskegee Avenue to address safety concerns.

The intersection of North Hill Street at Northside Drive and Tuskegee Avenue is currently considered a single intersection, controlled with a traffic signal. However, Northside Drive and Tuskegee Avenue do not align. Northside Drive is located just north of Tuskegee Avenue. It is recommended that Northside Drive and Tuskegee Avenue be realigned to a single through-street as redevelopment occurs. This will provide better east-west connectivity to Atkinson Elementary School and the commercial node at Northside Drive. A design-level traffic operational study should be conducted to identify need for turning lanes. Upgrades to the traffic controls and sidewalks are assumed. An intersection improvement at this location has been previously identified in the City of Griffin 2024 Comprehensive Plan.

Extend Bourbon Street to Elm Street to provide east-west roadway connection from Study Area to northwest Griffin.

As noted, the east-west roadway connections decrease north of West Cherry Street. To provide better roadway connectivity and accessibility, it is recommended that Bourbon Street be extended to Elm Street. This extension will provide access to northwest Griffin and the area around the University of Georgia Griffin Campus. A connection to Elm Street was identified because there is an existing rail crossing at Elm Street and Experiment Street which allows a greater east-west connection. The intersection of Elm Street at Experiment Street is identified a one of the intersections slated for improvement in project SP-069B: Spalding County Intersection Improvement Program, Phase II. The roadway concept includes two, twelve-foot vehicle lanes and a six-foot sidewalk on one-side of the street.

Bicycle and Pedestrian Alternatives in the Supplemental Study Area

Overall, the Study Area experiences significant pedestrian demand. One-quarter of the households within the Study Area lack private vehicles. A number of educational, shopping, and recreational destinations in the Study Area are currently underserved by pedestrian facilities. No marked bicycle facilities currently exist. The recommended pedestrian improvements include addition of sidewalks to streets providing connections to area destinations. Sidewalks on North Hill Street are included as individual recommendations; however, the project upgrade of North Hill Street from East/West Chappell Street to East McIntosh Road also includes sidewalks and bicycle lanes. The pedestrian and bicycle recommendations support City of Griffin and Spalding County transportation goals.

Fill gap in sidewalk network on east side of North Hill Street from East Cherry Street to Blanton Avenue.

Although the sidewalk network is fairly complete south of Tuskegee Avenue, a few gaps exist. One gap is on the east side of North Hill Street from East Cherry Street to Blanton Avenue. A six-foot sidewalk should be constructed to fill this gap to support pedestrian connectivity to Fairmont Park and nearby schools. The sidewalk should be set back from the roadway edge and separated with a landscaped buffer. The planning concept includes addition of curb and gutter to North Hill Street.

Other Bicycle and Pedestrian Alternatives

Fill gap in sidewalk network on west side of North Hill Street from Kentucky Street to Northside Drive.

Another gap in the sidewalk network exists on the west side of North Hill Street from Kentucky Street to Northside Drive. A six-foot sidewalk should be constructed to fill this gap to support pedestrian connectivity to Fairmont Park and nearby schools. The sidewalk should be set back from the roadway edge and separated with a landscaped buffer. The planning concept includes addition of curb and gutter to North Hill Street. This project should be completed in conjunction with the project to add sidewalks from Tuskegee Avenue and Northside Drive to Dobbins Mill Road/Dundee Lake Road.

Add sidewalk to one side of Bourbon Street and Elm Street in conjunction with the Bourbon Street Extension project.

At the time of the extension of Bourbon Street to Elm Street, sidewalks should be added to the existing sections of Bourbon Street and Elm Street to provide consistent sidewalk connectivity. The sidewalk would extend on Bourbon Street from North Hill Street to the extension and on Elm Street from the extension to W. Quilly/RR Avenue West. The concept consists of one six-foot sidewalk set back from the roadway with a landscape buffer, curb and gutter.

Add sidewalks to both sides of North Hill Street from Tuskegee Avenue and Northside Drive to Dobbins Mill Road/Dundee Lake Drive.

Very few sidewalks exist north of Tuskegee Avenue. Sidewalks are needed along the entire length of the North Hill Street extending from Tuskegee Avenue and from approximately 325 feet south of Northside Drive to Dobbins Mill Road/Dundee Lake Road to provide pedestrian connectivity to local destinations. Six-foot sidewalks should be added on both sides of the street. The sidewalk should be set back from the roadway edge and separated with a landscaped buffer. The planning concept includes addition of curb and gutter to North Hill Street. This project should be completed in conjunction with the project to add a sidewalk from Kentucky Street to Northside Drive.

Develop a multi-use trail connection from Jordan Hill Walking Trail and Jordan Hill Elementary to Dundee Lake Park on Dobbins Mill Road and Dundee Lake Road.

A multi-use trail connection is recommended to connect two recreational facilities, Jordan Hill Walking Trail and Dundee Lake Park, and Jordan Hill Elementary School. The concept consists of a 12-foot multi-use trail parallel to Dobbins Mill Road and Dundee Lake Road, separated with a landscape buffer. This concept supports City of Griffin and Spalding County goals to increase trail connections. The trail could eventually connect to walking trails planned in the Dundee Lake Park Master Plan and proposed multi-use trail system along the rail line.

Add sidewalks to East McIntosh Road, Northside Drive, and Lincoln Road to provide needed pedestrian connections to local destinations.

Addition of six-foot sidewalks is recommended for both sides of East McIntosh Road from North 9th Street to Greer Road, for both sides of Northside Drive from North 9th Street to North Hill Street, and for one side of Lincoln Road from Talladega Avenue to Tuskegee Avenue. The planning concept includes curb and gutter. The sidewalk should be set back from the roadway edge and separated with a landscaped buffer.

Transportation Policy Recommendations

In addition to specific project recommendations, transportation strategies and policies have been identified to provide guidance for the North Hill Street Corridor Study Area, as follow:

- New development within and around the Study Area should continue connected street patterns consistent with the smaller block network on the south end of the Study Area, minimizing cul-de-sac streets.
- Opportunities to provide additional east-west street connections should be identified as development occurs.
- The City of Griffin, Spalding County, and the Griffin-Spalding County School District should consider the applicability of Safe Routes to School programs for schools within the Study Area.

Transportation Project Recommendations

From the list of projects both within and outside of the Supplemental Study Area, three key recommendations are provided relevant to said Study Area.

- Realign the intersection of North Hill Street at Blanton Avenue and North 6th Street to address safety concerns.
- Upgrade traffic signs and pavement markings throughout Study Area to current standards.
- Fill gap in sidewalk network on east side of North Hill Street from East Cherry Street to Blanton Avenue.

Implementation Program

The implementation program was developed to identify resources and actions necessary to implement recommended projects. The implementation program includes project costs, funding sources, agency responsibilities, and recommended time periods. Costs for projects were estimated through use of the GDOT guidelines and professional practice. The costs do not include utility fees. It should be noted that the project costs do not reflect the detailed field verification and inventory required for engineering-level design; rather estimates consider sketch analysis using data collected and reviewed via GIS and aerial photography. The implementation program is shown in Table 8.

The North Hill Street Corridor study provides a guide for future transportation improvements. An important ongoing task is to ensure the plan and program continues to meet the needs of Spalding County and the City of Griffin. Ongoing plan activities include:

- Coordinating with ARC and GDOT to advance projects in future RTP updates;
- Ensuring projects are implemented in a logical sequence to maximize benefits and utilize scarce resources efficiently;
- Continuing intergovernmental coordination activities to ensure transportation projects, policies, and programs are compatible; and

Table 9: North Hill Street Corridor Supplemental Study Recommended Transportation Projects

Project Description				Project Type	Existing Dimensions (Estimated)*		Concept Dimensions	
Location	Description	From	To		Right-of-Way	Roadway Width	Desired Cross-section	Length (Feet)
N. Hill Street at Blanton Avenue and N. 6th Street	Realign intersection to address safety concerns; Close N. 6th Street; Connect Realty Street to Quincy Avenue	Realty Street	Blanton Avenue	Intersection Realignment	N. Hill - 60'		Two-lanes	
LCI Supplemental Study Area intersections on Chappell Street	Upgrade traffic controls for pedestrians, add pavement markings	North Hill Street	North 6th Street	Traffic Operations	n/a	n/a		
N. Hill Street	Fill gap in sidewalk network on east side of street	Cherry Street	Blanton Avenue	Pedestrian Facility	60' to 100'	20/25'	One, 6' sidewalk	1,672

*Note: Existing right-of-way and roadway width was estimated using GIS for planning purposes only. No field verification was undertaken.